

High Halstow

Design Code

October 2022

1172-URBED-RP-L-Design Code-High Halstow

This report has been produced by URBED for the High Halstow Parish Council.

Original: 15-10-2021

Final Draft: 25-10-2021

Final Draft (updated) 27-10-2021

Design Code Update 23-05-2022

Design Code Update 26-07-2022

Final Design Code: 07-10-2022



URBED (Urbanism Environment Design) Ltd.

a. 5th Floor, 10 Little Lever Street, Manchester, M1 1HR

t. +44 (0)161 200 5500

w. www.urbed.coop



Contents

- 01 Introduction** 5
 - 1.0 Introduction..... 6
 - 1.1 High Halstow..... 8

- 02 High Halstow Parish Area Design Code** 11
 - 2.0 Parish Design Code..... 12
 - 2.1 Land to the East of High Halstow..... 16

- 03 Area Types Design Code** 18
 - 3.1 Area Type 1: Village Centre..... 21
 - 3.2 Area Type 2: Village Fringe..... 27
 - 3.3 Area Type 3: Rural-Edge..... 33
 - 3.4 Area Type 4: Rural..... 38

- 04 Appendix** 41
 - 4.1 Design Code Precedents..... 42
 - 4.2 Consultation Process..... 48
 - 4.3 Reg 14. Consultation Responses..... 50

Introduction

This section introduces the High Halstow Design Code, its purpose and the areas it applies to.

01

1.0 Introduction

Purpose of the Document

High Halstow Parish Council has produced this Design Code to support the High Halstow Neighbourhood Plan policies to ensure that good design is integral to all new developments in the Parish.

This document guides designers and developers to meet the aspirations of the existing High Halstow community and ensure that design proposals are of high quality and integrate well with the existing village and surrounding landscape.

The Design Code sits alongside the High Halstow Parish Neighbourhood Plan, consolidating its vision and design principles to provide specific commitments for development approvals.

Structure of the Document

In January 2021, the Government published The National Model Design Code NMDC (2021), which provides detailed guidance on designing codes, guides and policies to promote successful design.

The High Halstow Design Code follows the National Model Design Code process of understanding the existing context of the area and setting design codes across each of the key themes:

- Movement;
- Nature;
- Built Form;
- Identity;
- Public Space;
- Use;
- Homes and Buildings;
- Resources; and
- Lifespan

The design codes are listed in a set of tables throughout this document, and each code sets a fixed rule or

parameter with an acceptable range for compliance to which future development must adhere.

The NMDC suggests that coding is done at two levels. The first of these includes rules that apply to all development across the Parish and is set out in part 2 of this document.

The second level includes rules that relate to specific areas, which the NMDC calls 'Area Types'. These are areas designed to have a particular character and are inspired by the best parts of the surrounding area. This code uses four Area types:

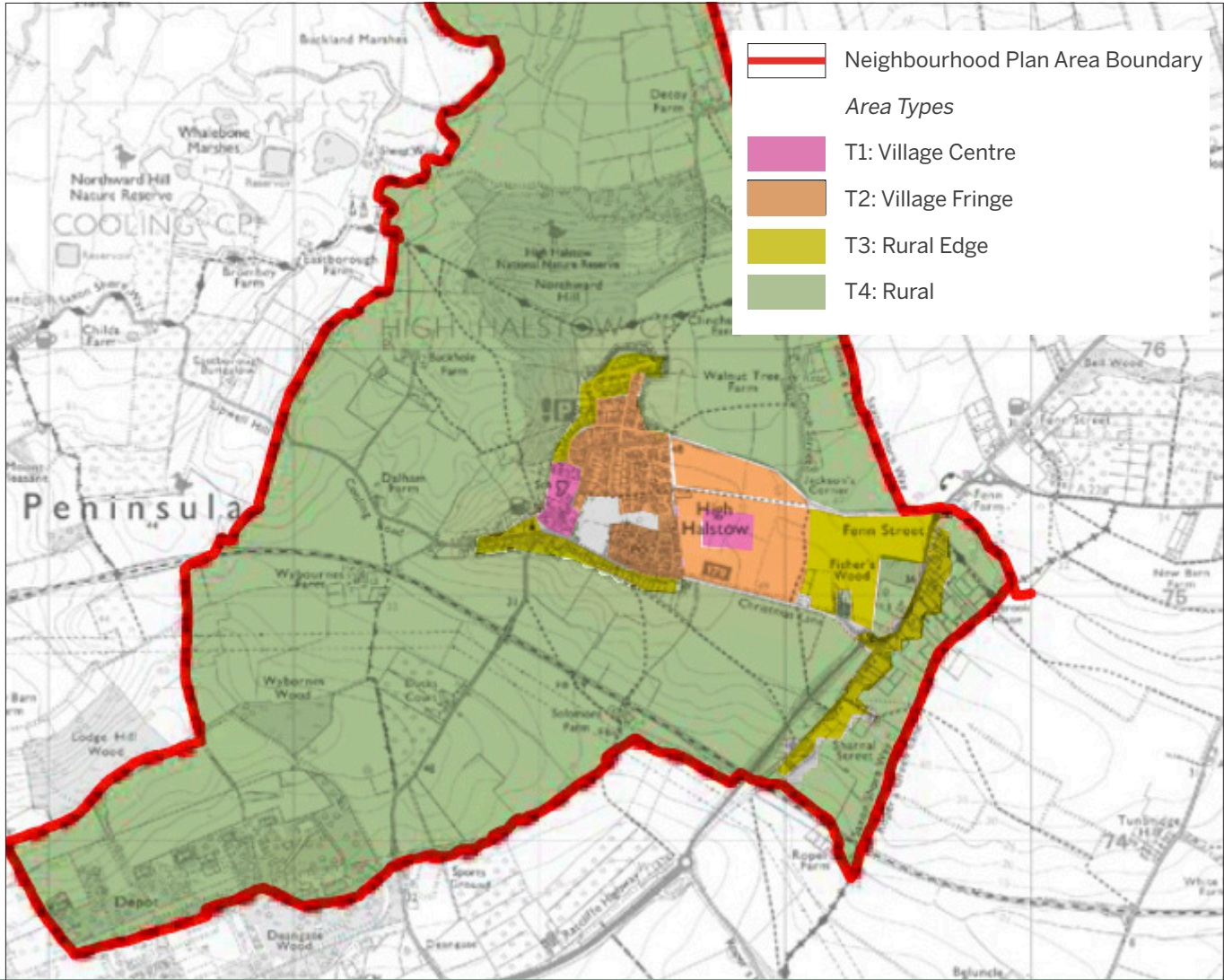
- Village Centre
- Village Fringe
- Rural Edge
- Rural

The NMDC also sets out how a framework plan should be prepared for new development sites to show how the rules will be applied. This code, therefore, also includes a framework plan for the main development site in High Halstow, which has a draft allocation in the local plan for 700 new homes.

Understanding the Context

The guidance in this code is based on national guidance as set out in the National Design Guide and the NMDC Guidance notes. However, it is also based on a detailed baseline of the local context in High Halstow, available as a separate document and a Character Area Assessment of High Halstow and other Kent Villages as set out in the Appendix of this document.

The code has also been informed by the comprehensive historical assessment of the Hoo Peninsula, produced by Historic England in 2014.



Neighbourhood Plan Area & Area Types

High Halstow Parish Council

High Halstow Neighbourhood Plan
2020-2037
Draft (Regulation 14) Version, May 2021

(Image source © Google Earth)

High Halstow Neighbourhood Plan, 2020-2037, (2021)

RESEARCH REPORT SERIES no. 53-2014

HIGH HALSTOW, HOO PENINSULA, KENT
HISTORIC AREA ASSESSMENT

Joanna Smith and Jonathan Clarke

ASSESSMENT

ENGLISH HERITAGE

High Halstow Historic Area Assessment, (2014)

1.1 High Halstow

High Halstow Parish Area

The Parish area of High Halstow is located approximately 46km east of London and 8km north of Medway Towns on the northern side of the Hoo Peninsula. The Neighbourhood Plan Area covers the extent of the High Halstow Parish Area, shown bounded in red on the adjacent map.

The area sits within the unitary authority of Medway Council and shares its borders with the settlements of St Mary Hoo to the east, Hoo St Werburgh to the south, Cooling to the west and bordered by the Thames River to the north.

The Neighbourhood Plan Parish area includes the village of High Halstow, several hamlets along the A228 road, and many dispersed farmsteads.

The Parish is served by the A228, the Ratcliffe Highway, that runs along the spine of the peninsula and passes to the south of High Halstow.

The village is served by Christmas Lane, Britannia Road, Cooling Road and Dux Court Road, all of which are narrow lanes. It currently has limited access to the railway network, but there are plans to reopen the goods line running south of the village to passenger services that will include a new station.

There are plans for major housing development on the Hoo Peninsula, including up to 12,000 new homes, much of them in a new settlement at Hoo St. Werburgh that will come to within less than a mile of High Halstow. The council has secured £170M from the Housing Infrastructure Fund HIF to provide the infrastructure needed to serve these new homes.

Land to the East of High Halstow

As part of the plans for the Hoo Peninsula, the land to the East of High Halstow has been identified for 700 new homes by Medway Council in the emerging Local Plan.

This is being considered as part of the masterplanning work being undertaken for Medway Council for the wider HIF-funded growth and is being brought forward by the housebuilder Redrow.

The Parish Council has made clear in the Neighbourhood Plan submission that they oppose the development of this land in principle, and certainly before the infrastructure investment has been made. Additionally, the Parish Council believe that the development of housing in rural areas necessitating large scale infrastructure improvements is inconsistent with climate change concerns.

However, they have also taken a pragmatic stance, recognising that the site is likely to be allocated in the local plan. They have therefore produced this design code to ensure that, if the development comes forward, it is as good as it can be, that it responds to the character and identity of the existing settlement and meets the community's needs.

Application for development of the site will be considered premature ahead of the new Medway Local Plan being adopted and the HIF strategic infrastructure being in place.



River Thames

High Halstow Parish

Cliffe

Cooling Parish

Allhallows

St Mary Hoo Parish



High Halstow

Cooling

Cliffe Woods

A228

River Medway

	High Halstow Neighbourhood Plan Boundary
	Land to the East of High Halstow

Neighbourhood Plan Study Area

High Halstow Parish Area Design Code

This section sets out design codes that apply to new development proposals across the whole Parish area.

02

2.0 Parish Design Code

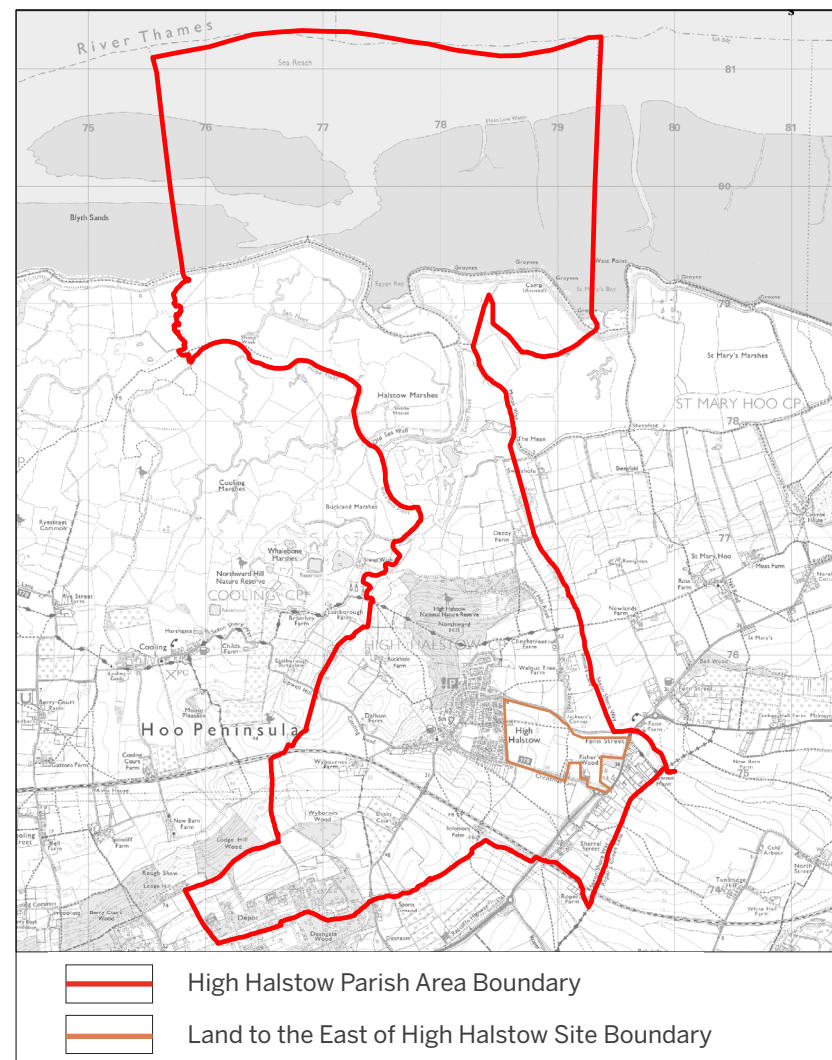
High Halstow Parish Area

Some principles are applicable to all development regardless of which Area Type they are in. The page opposite, therefore, sets out the rules that apply to all proposed development in the High Halstow Parish area, including the Land to the East of High Halstow as shown on the adjacent plan.

The design codes and their themes align with the following policies in the Neighbourhood Plan:

- Nature and Public Space: The code relates to Policy HH E6: New green spaces and Policy HH E1: Natural Environment;
- Homes and Buildings, Resources and Lifespan: The Code relates to Policy HHE8: Towards zero carbon development;

- Movement, Built Form and Public Space: The code relates to Policy HHM5: Street design, HHM1: Green routes and HHM3: Bus services, routes and infrastructure;
- Built Form, Identity, Use, Homes and Buildings, Resources and Lifespan: The Code relates to Policy HHPQ1: Design;
- Uses: The Code relates to Policy HHC1: Village Centre and facilities and Policy HHC2: Education.



High Halstow Parish Area

Movement

Objective: All new development must create a walkable, safe network of streets that discourage car use and encourage walking and cycling:

M01 - Streets:

New streets must link at either end to other streets and allow free access for walking and cycling. Car access may be restricted within the street network so that not all streets allow through traffic.

M02 - Street Hierarchy:

All streets must be allocated to a level of the street hierarchy. Each street must follow the appropriate design guidance in section 3 of this code.

- High Streets must provide for through traffic and local shops and services.
- Secondary Streets must give access into each part of the neighbourhood.
- Tertiary Streets must allow only for frontage access and may include home zones.

M03 - Speed Restrictions:

All new streets must be designed for a 20mph speed limit.

M04 - Public Transport:

All new development must be within a 5-minute walk of a bus stop with a service of at least four buses an hour.

M05 - Active Travel:

New development must incorporate clear cycle and footpath routes and contribute to the improvement of cycle and footpath routes in the wider area, particularly to the station. These could be wide, well surfaced, safe and well lit. All new homes must make provision for at least 2 cycle parking spaces, including apartments.

M06 - Parking

Parking provision must be in line with the local plan standard. On High Streets and Secondary Streets, on-plot parking must not be in front of the building. On-plot, on-street and communal parking bays, for households must include electric vehicle rapid charging points.

M07 – Servicing:

New development must integrate the requirements of utility providers and refuse collection and emergency access without compromising the quality of place by obstruction of movement or visual intrusion. Communal bin stores or underground waste storage bins must be provided for new flats.

Nature

Objective: Development must enhance the natural environment by creating a network of green spaces and enhancing biodiversity and the wellbeing of residents.

N01- Open Space Provision:

Development of a scale that triggers the provision of new green space must be provided in line with standards established by Medway Council and must provide at least 1.6ha of open space per 1000 people.

N02 - Play Provision:

All homes must be within 100m of a local area of play, 400m of a local equipped area of the plan and 700m of a Neighbourhood equipped area of play (see national Model Design Code Guidance notes for definitions).

N03 - Open Space Design:

Spaces must be designed to be overseen from surrounding buildings, streets and public spaces - open spaces must never be enclosed by back gardens. All new open spaces must be designed in line with the guidance in N.1.iii of the NMDC Guidance Notes.

N04 - Drainage:

Schemes must incorporate Sustainable drainage systems that achieve greenfield run-off rates while enhancing biodiversity in line with the guidance in N.2.ii of the NMDC Guidance Notes. New development must include the use of permeable paving within hardscaping.

N05 - Biodiversity:

Schemes must achieve a 10% increase in biodiversity as measured by using the Natural England Biodiversity Metric 3.0. Schemes must be designed to enhance biodiversity, including the retention of existing trees, hedges and habitats, the reduction of light pollution and the creation of new habitats, green roofs and ecological networks.

N06 - Street Trees:

All new streets must incorporate street trees within the public realm at spacings of at least one tree every 20m.

N07 - Allotments:

Development of the Land East of High Halstow site must provide at least 0.66ha of land for allotments with a minimum of 20 plots.

Built Form

Objective: To create a characterful village environment with well-proportioned streets and public spaces.

B01 - Public and Private:

New development must create a clear separation between the public fronts of houses that face onto streets and the private rear gardens.

B02 - Built Form:

All new homes must follow the density guidance, building line, and heights for the Area Types set out in section 3.

Identity

Objective: All schemes must be designed to respect and enhance the character of High Halstow and the villages of the Hoo Peninsula.

I01 - Sense of Place:

All schemes must be designed to enhance local character and legibility by making use of local materials and detailing.

I02 – Masterplan:

New development must be guided by a masterplan that provides a long term framework for the entire build-out period. The masterplan must be designed to create a sense of place and identity, including the identification of views and vistas, landmarks and corner features. See para I.1.iii of the NMDC Guidance Notes.

I03 - The Identity of Buildings:

All buildings must take account of the principles set out in Para I.2 of the NMDC Guidance Notes.

I04 - Public Art:

All new schemes must include a strategy for the provision of public art.

Public Space

Objective: Development must create and enhance an attractive, safe and inclusive network of public spaces, including streets, squares and green spaces.

P01 - Streets:

These must be designed as public spaces capable of accommodating a range of uses (rather than just the car).

P02 - Street Design:

The design of each street must relate to its place in the street hierarchy and the Area Type.

P03 - Home Zones:

Tertiary level streets must be designed using home zone principles as set out in Para P.2.iii of the NMDC Guidance Notes.

P04 - Safety:

All streets, squares and green spaces must be fronted onto and overlooked by development. Streets and public spaces must be designed for natural surveillance from other street users and surrounding buildings. Secured by Design principles as set out in Para P.3.i of the NMDC Guidance Notes must be followed.

Use

Objective: To create a diverse neighbourhood with a mix of people and local services and facilities.

U01 - Housing:

New schemes must include a mix of housing in terms of type and tenure. They must meet Medway's affordable housing standard and include a diversity of house types in terms of form (detached, semi-detached and terraced, apartments, bungalows, elderly persons accommodation, and supported/assisted living accommodation) and size (number of bedrooms).

U02- School:

The school must be integrated into the village relating to both the existing and new development and catering for joint use of its facilities.

U03 - Shops:

The new village centre must include a row of shop units, with at least one to be a café, second providing a mini supermarket and a third, providing a post office with units let on terms that encourage their occupation by local businesses and to provide active frontages.

Homes

U04 - Community Facilities:

New development must enhance and expand the provision of local services such as doctors surgeries, community space and childcare facilities.

U05 - Homeworking:

All new homes must be designed to facilitate home working in terms of internal space provision and broadband connectivity and for developments over 80 units must provide an on-site homeworking hub.

U06 – Self/Custom Build:

Opportunities to incorporate self and custom build plots in new development are supported and must be in line with HH's Neighbourhood Plan Policy HHPQ6: Self and custom build housing. Plot passports must be prepared for approval by the LPA at the outline planning application stage, setting out the design of plots and their placement and integration into other development. All plots must be provided with connections to utilities and communications infrastructure. For the land east of High Halstow, a minimum of 5% of the plots must be self/custom build, and self-build areas are to be no smaller than ten plots in size.

Objective: All new housing will be well designed and suitable to the needs of its occupants.

HB01 - Housing Design:

The Nationally Described Space Standards will apply to all new housing and must provide a range of housetypes including homes to accommodate young families.

HB02 - Daylight:

Housing must be designed to maximise internal daylight while avoiding overheating, and all homes must be designed as dual aspect.

HB03 - Privacy Distances:

A minimum privacy distance of 17m will apply between windows to the rear of properties. There will be no privacy distance at the front of a property.

HB04 - Gardens:

All new homes must have access to external amenity space. For houses, this must be at least as large as the footprint of the house. Water butts must be provided in gardens of new households to collect rainwater.

Resources

Objective: New development must be zero carbon ready and facilitate a sustainable future for the village.

R01 - Zero Carbon:

All new houses must be zero-carbon ready in terms of day-to-day resource use and follow the 2025 Future Homes Standard and include fabric efficiency, no-fossil fuel-based heating/hot water systems and include solar panels, even if building starts before 2025.

R02 - Passive Design:

Housing design must take account of building orientation. The internal arrangement and glazing ratio of homes should depend on the orientation of the home.

R03 - Embodied Energy:

New homes must include an assessment of embodied energy and include a plan to reduce it by 30% compared to a typical new build dwelling.

R04 - BREEAM Rating:

New non-residential buildings must achieve at least a BREEAM 'Outstanding' rating.

R05 - Water Usage:

All new homes must be designed to achieve water usage of 110 litres per person per day.

Lifespan

Objective: To ensure that all new development is well managed and incorporates the views of the new and existing community.

L01 - Management Plan:

All new development must include a management plan setting out adoption arrangements and management arrangements for non-adopted areas and facilities, including the potential for community management.

L02 - Community Participation:

In addition to consulting High Halstow Parish Council (which is already a statutory consultee on planning applications), a separate consultation will be required with the community on all schemes throughout the development process up to and including management for new developments of a size of 50 or more units.

2.1 Land to the East of High Halstow

The Parish Council maintains that development of the Land East of High Halstow is unsustainable, especially if brought forward before the new Medway Local Plan (2019 to 2037) is adopted and before appropriate infrastructure improvements to the site and surrounding areas are made.

The Parish Council acknowledges, however, that the site could come forward within the new Local Plan and if so, want to ensure that development is brought forward to the highest standards and quality and will meet the existing and future needs of High Halstow Village.

Accordingly, the Parish Council have produced a Masterplan Framework and specific rules for development of Land East of High Halstow.

Land East of High Halstow Masterplan Framework

In addition to the general rules set out in the previous section, the code includes specific rules for different character areas - called Area Types in the NMDC.

These are set out on the following pages and can be applied to the existing parts of the village. However, their main role is to set detailed rules for the site known as the land East of High Halstow to the South of Britannia Way and North of Christmas Lane.

In order to apply these rules it is necessary to have a framework plan to show how they apply to the new development. The framework plan opposite has been designed to do this.

This has been developed in discussion with the developers for the site and their masterplanners to produce a plan that all parties can hopefully agree.

Our understanding is that this is the case except for point 8 described below:

Key Design Principles

- 01 Development that integrates into and relates to the existing Village of High Halstow, retaining its identity as a rural settlement;
- 02 A new network of streets with a clear street hierarchy which connects with the existing settlement via Britannia Road and Christmas Lane;
- 03 A bus route connecting the existing settlement village centre with the new proposed village centre and down to the proposed new railway station to the south of the Parish;
- 04 A new village centre, with a new primary school and a mix of shops and community uses. The new primary school must include parking for its ancillary staff and align with Medway's parking standards.
- 05 The creation of three Area Types that vary in built-form, street typology, densities and identity;
- 06 Provision of new open parkland space, integrating SuDs and new tree planting and allotments;
- 07 A substantive buffer between High Halstow and the Ratcliffe Highway with extensive tree planting and enhancement of the landscape character;
- 08 A new main access road to the village centre direct from the roundabout on the Radcliffe Highway. This potentially allows part of Christmas Lane to be closed to car traffic. This will create a strong route to the proposed new station;
- 09 Provision of green spaces, designed to be actively used, that follow desire lines and provide for a range of recreational uses, including opportunities for play and growing spaces;

- 10 Ensure maximum connectivity and permeability between the two village centres and improvements to the movement network that provides an opportunity for people to walk and cycle across the site safely;
- 11 Safeguards and enhances the existing woodland to the east of the site and provides new tree planting; and
- 12 New street tree planting on all new primary and secondary streets.
- 13 New car parking area located within the Village Centre area.



Framework Plan



Area Types within High Halston Village

- Village Centre
- Village Fringe
- Rural Edge

Area Types Design Code

This section sets out design codes that apply for within four different area types across the Parish area.

03

3.0 Area Type 1: Village Centre

Location

The Village Centre Area Type relates to both the new and existing village centres. The aim is to create twin centres for the village, the old centre with the pub, church and community halls and the new village with the new school, shops and other facilities.

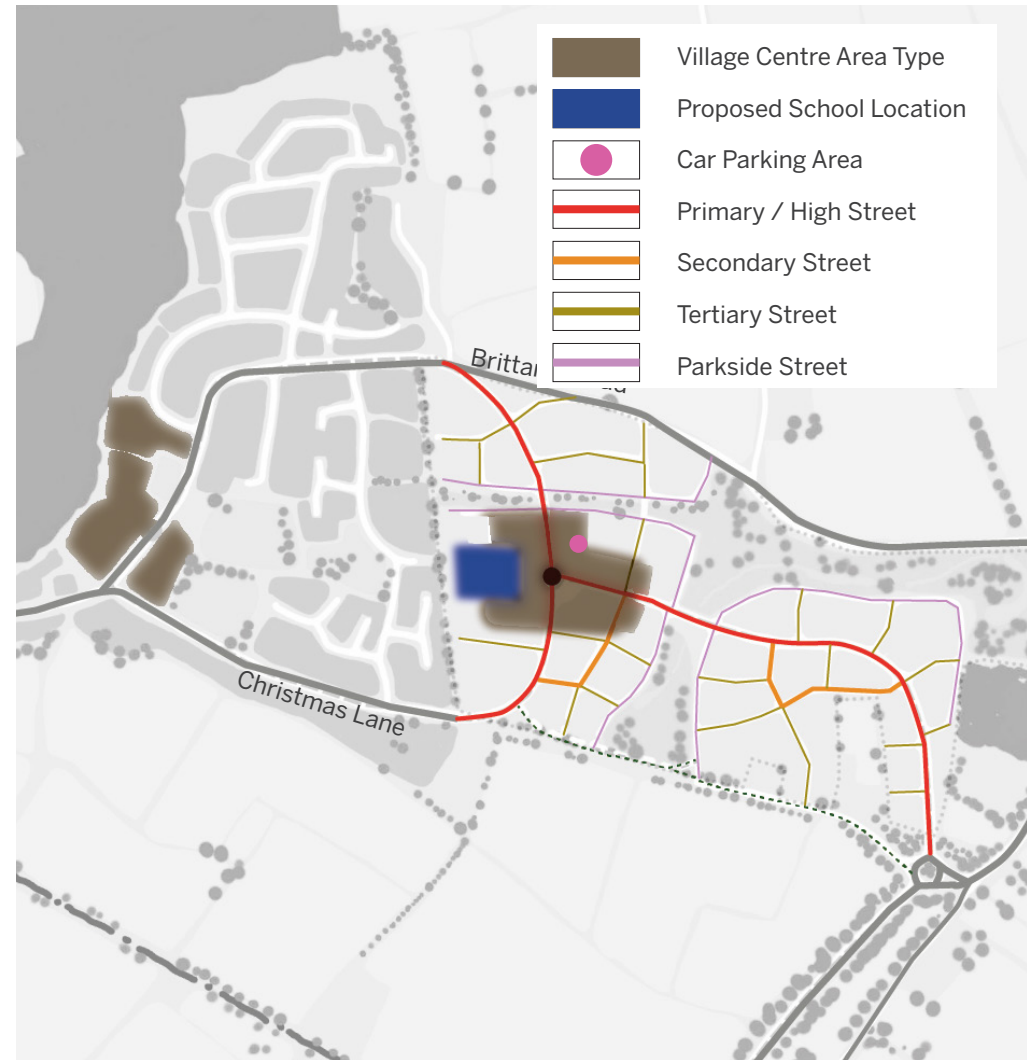
The new village centre is at the junction of two proposed high streets and will be built around a public square bounded by the school and a set of local shops and services.

Character

The Village Centre Area Type is based on an analysis of the centre of other nearby villages like Cliffe. The centre of the village covers a small area but is more urban in character with the following characteristics:

- Housing density of between 50-60 dwellings per hectare.

- Predominant heights of 2 storeys but with some three-storey buildings.
- Buildings that generally join their neighbours, creating terraces.
- Buildings that closely follow a clearly defined building line.
- Buildings that are set back no more than a meter from the back of the pavement.
- Mixed-use development with apartments or offices over ground floor retailing, cafes or other facilities.
- Distinctive buildings and public art that mark the centre of the village and create landmarks seen along the approaching roads.
- High-quality public realm that explores a shared surface treatment on the main square.



Village Centre Area Location

Village Centre Coding

The following design codes set out the regulatory rules for the Village Centre area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Village Centre.

VC 1 - Streets:

Development in the village centre will define the primary, secondary and tertiary streets shown on the plan on page 21. The high street will make provision of a bus service and bus stop in the village square.

VC2 - Cycle Parking:

To be provided based on 2 spaces per dwelling and 3 visitor spaces per shop plus additional spaces for the school. The residential provision is to be provided in courtyards while the visitor cycle parking must be on-street.

VC3 - Car Parking:

To be provided based on 1 space/ residential unit with 50% additional visitor parking. This parking must be provided as on-street parking bays or in rear courtyards. A parking courtyard must be provided within the village centre area for visitor parking.

VC4 – Green spaces:

The main open spaces (the new square and the school grounds) must be designed to maximise biodiversity.

VC5 – Housing Density:

New housing must be built at net densities of 50-60 dwellings/hectare

VC6 – Party Wall:

Most buildings (75%) must be joined to their neighbours, creating a party wall, with the exception of elevations facing intersecting streets.

VC7 - Form:

Development must create a varied and fine-grained development form with an informal layout, a variety of building heights, narrow plots and a range of materials. Buildings on corner plots must turn the corner, providing

distinctive features with windows overlooking the public realm.

VC8 - Building Line:

A clear building line can be set for development. This must be set no more than 1m from the back of the pavement. Buildings must project forward and back from the building line by no more than 0.5m.

VC9 - Height:

The eaves height of new buildings must not exceed 10m or be less than 6m. The total height of buildings must be no more than 3m above the eaves height and most buildings will have pitched roofs.

VC10 - Boundary Treatment:

Where small front gardens are created, a dwarf wall, white picket fence or railings can be used as a boundary treatment.

VC11 - Identity:

Buildings, whether contemporary or traditional design should respect local vernacular:

- Ground floors must be differentiated either architecturally or by materials.

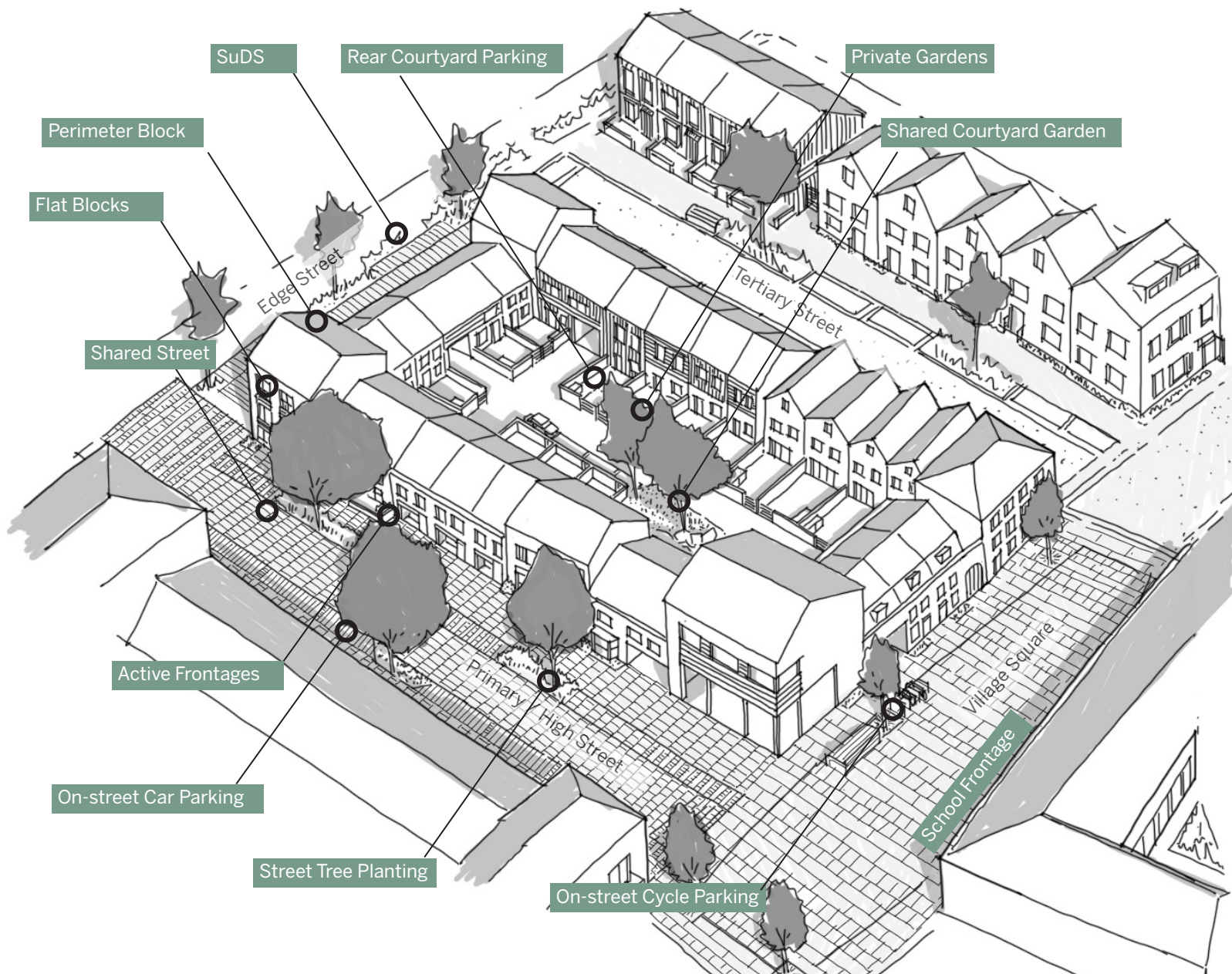
- At least 50% of entrances must be marked with porches.
- Rooflines must be varied with some gables facing the street.
- A variety of materials must be used, which could include red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Windows should be orientated vertically with the use of bay windows.

VC12 - Privacy distance:

A minimum privacy distance of 17m will apply between windows to the rear of properties.

VC13 - Public Art & Wayfinding:

Development must provide information boards with local information of both village centres and key locations and distances marked on them. Development must provide a palette of bespoke wayfinding elements including public art and sensory elements within the public realm to reflect the distinctiveness of the village centre area and enhance the experience of moving within it. Public art must be integrated within the village square.



SuDS

Rear Courtyard Parking

Private Gardens

Perimeter Block

Shared Courtyard Garden

Flat Blocks

Edge Street

Tertiary Street

Shared Street

Active Frontages

Primary High Street

Village Square

On-street Car Parking

School Frontage

Street Tree Planting

On-street Cycle Parking

VC14 - Public Space:

New public spaces must be designed to the specifications set out in the adjacent table.

VC15 - Mix of Uses:

The high street and square must incorporate a row of shop units. These will create an active frontage on at least 50% of the frontage.

VC16 - School:

The new school will be subject to these rules; it must follow the building line, face the square and take its main pedestrian access from it. There will be no stopping and parking permitted for vehicles in the village square in front of the school. Designated parking for the school, must be provided within the school site area.

VC14 - Public Space									
	Enclosure ratio	Width between buildings	Active Frontage	Building line compliance	Parking	Cycling	Footway	Street Trees	Tree Spacing
High Street	Min 1:2.8 Max 1:1	13-16.5m	50%	90%	One side of street	On shared surface	At least 2m/ shared surface	One side alternating	Min 12m Max 24m
Secondary Street	Min 1:2.6 Max 1:0.8	11.-13m	NA	80%	Both sides	On Carriageway	At least 2m	Both sides	Min 12m Max 24m
Tertiary Street	Min 1:2 Max 1:0.8	10-12m	NA	70%	Both sides	On Carriageway	Part of shared surface	Both sides	Min 12m Max 24m
Parkside Street	NA	NA	NA	70%	One side of street	On Carriageway	On shared surface	One side	Min 12m Max 24m

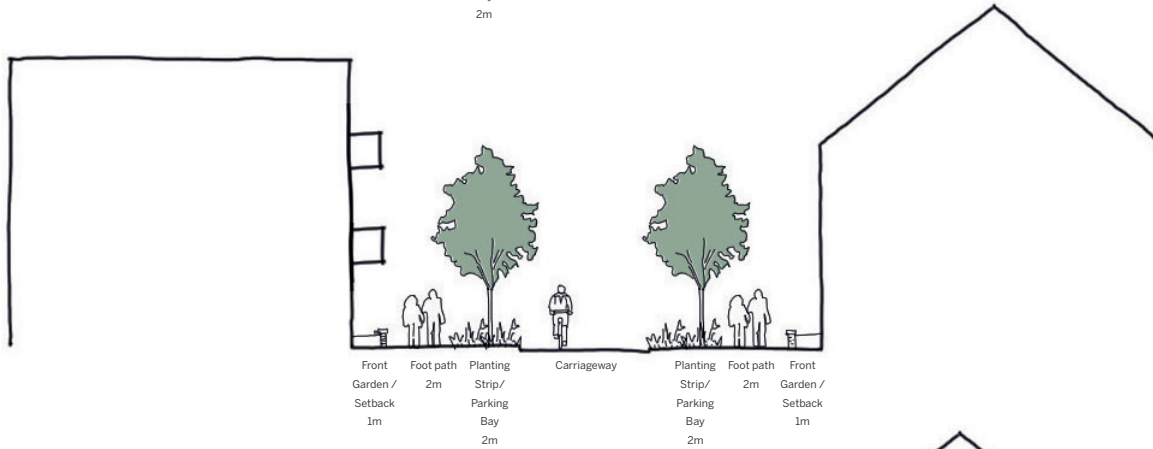
Village Centre
Village Square
Example Section



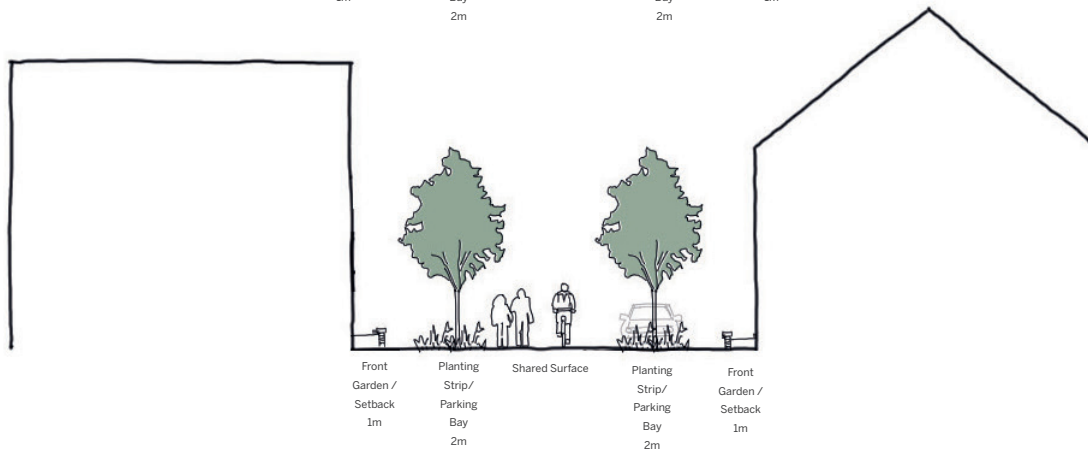
Village Centre
High Street
Example Section



Village Centre
Secondary Street
Example Section



Village Centre
Tertiary Street
Example Section



Cliffe, Reed Street - terraces, short frontages and sense of enclosure to the street



Precedent - Village Square in Lightmoor Village, Telford



Precedent - Urban Village Shared Street

3.1 Area Type 2: Village Fringe

Location

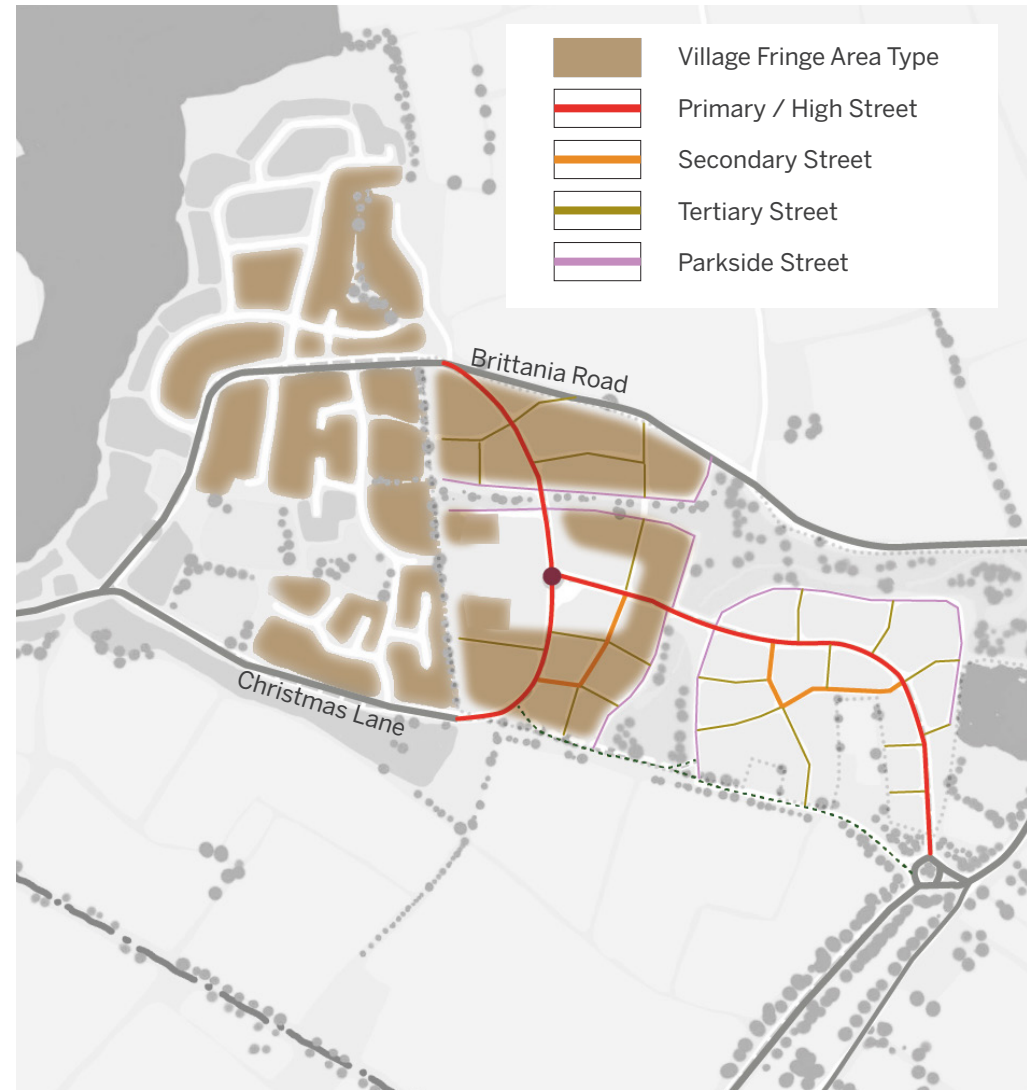
The Village Fringe Area Type includes the area around the new village centre next to the existing village. It also includes most of the existing village. The area includes high streets, secondary, tertiary and parkside streets, although the former will not include any shops.

Character

The character of the area is drawn, in part, from the existing development in High Halstow and other local villages. However, the aim of Area Types is to create a vision for how they should be in the future, rather than just replicating what exists.

The Village fringe will therefore include:

- Housing densities of between 30 and 40 dwellings per hectare.
- Predominant heights of 2 storeys.
- Buildings that are generally semi-detached and have short terraces and bungalows.
- Buildings that informally follow a building line.
- Buildings that are set back 2-5m from the back of pavement.
- Solely housing development but with provision for home working.
- Housing design that is a contemporary or traditional interpretation of the local vernacular.



Village Fringe Area Location

Village Fringe Coding

The following design codes set out the regulatory rules for the Village Fringe area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Village Fringe.

VF 1 - Streets:

Development in the village fringe will define the Primary, secondary, tertiary and parkside streets shown on the plan on page 27. The high street will make provision of a bus service.

VF2 - Cycle Parking:

To be provided based on 2 spaces per dwelling to be provided on-plot.

VF3 - Car Parking:

To be provided based on 1.5 spaces/ residential unit. One unit must be provided on-plot, with the remaining third being unallocated on-street parking. No more than 40% of the on-plot parking must be in front of the building line.

VF4 – Green spaces:

The garden and courtyard spaces must be designed to maximise biodiversity. This can include a variety of tree planting/shrub planting in front and back garden areas and tree/shrub planting and rain gardens within verges along the street.

VF5 – Housing Density:

New housing must be built at net densities of 30-40 dwellings/hectare.

VF6 – Party Wall:

At least 80% of housing must be joined to their neighbours on at least one side, creating a party wall.

VF7 - Form:

Development must create a varied and fine-grained development form with an informal layout and a range of materials.

VF8 - Building Line:

A clear building line must be set for development. This must be set 2-5m from the back of the pavement. Buildings can project forward and back from the building line by no more than 1m.

VF9 - Height:

The predominant building height must be 2 storeys with an eaves height not exceeding 7m. 2.5 storey buildings are, however, permitted with a maximum eaves height of 8m (accommodation partly within the roofspace). The total height of buildings must be no more than 3m above the eaves height, and most buildings will have pitched roofs.

VF10 - Boundary Treatment:

The boundary of front gardens should be marked by a hedge, timber fencing, cleft post or metal railings no more than 1m in height.

VF11 - Identity:

Buildings, whether contemporary or traditional design should respect local vernacular:

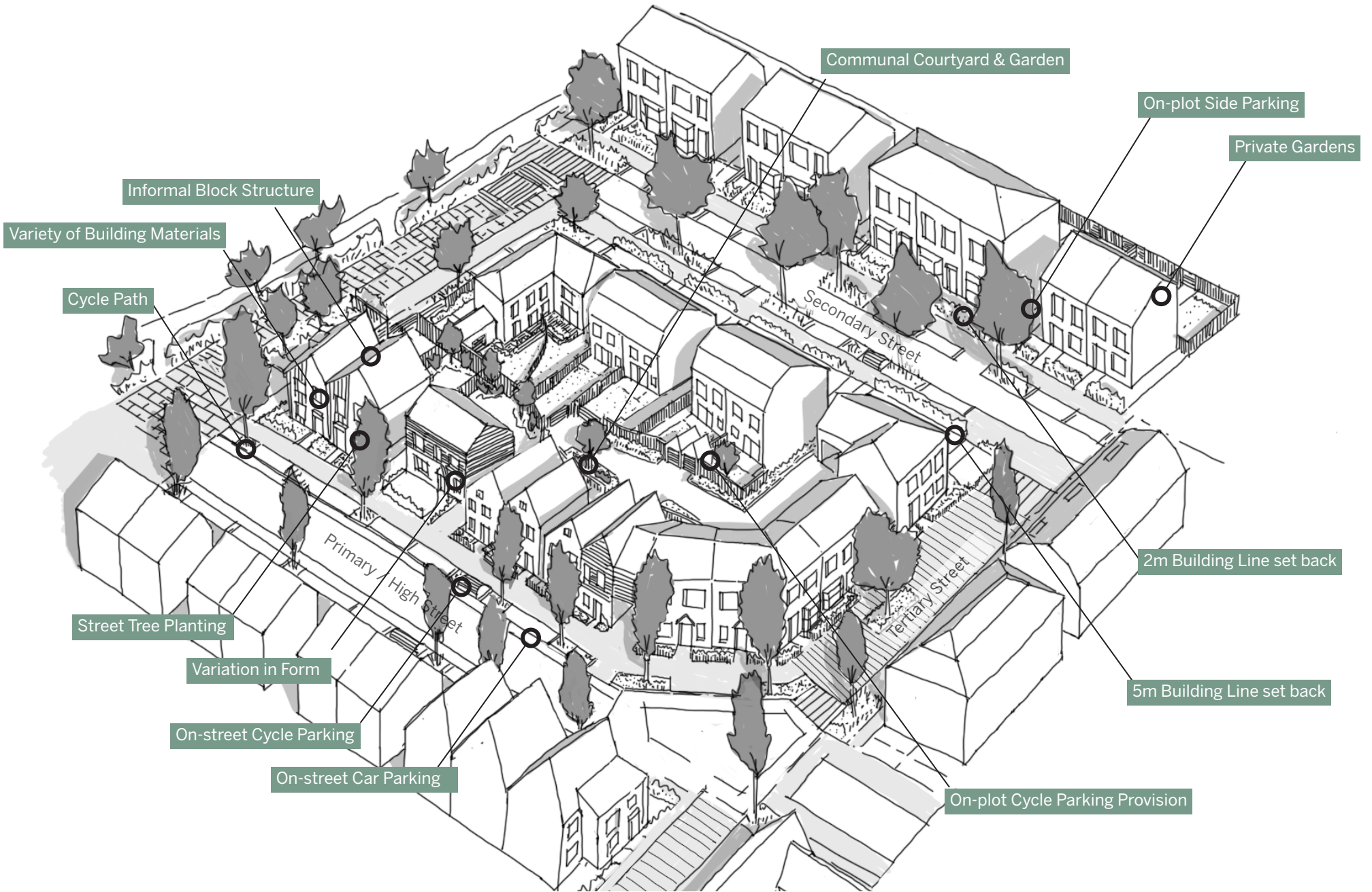
- At least 70% of entrances must be marked with porches.
- Rooflines must be varied, with some gables facing the street.
- A variety of materials must be used, which could include red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Windows should be orientated vertically with the use of bay windows.

VF12 - Privacy distance:

A minimum privacy distance of 20m will apply between windows to the rear of properties.

VF13 - Public Art & Wayfinding:

Development must provide a palette of bespoke wayfinding elements, including public art and sensory elements within the public realm, to reflect the distinctiveness of the village fringe area and enhance the experience of moving within it.



Village Fringe Area Character

VF14 - Public Space:

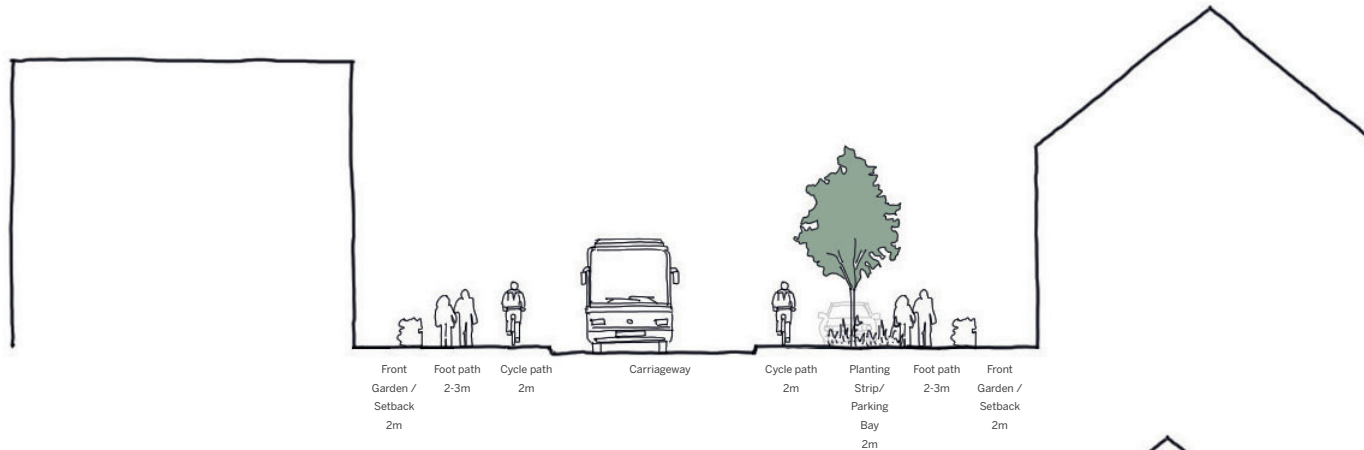
New public spaces must be designed to the specifications set out in the adjacent table.

VF15 - Mix of uses:

This area will be predominantly residential; however, development plots must provide enough rear amenity space for the option to accommodate a rear home-working studio whilst leaving a minimum area the size of the dwelling footprint for private amenity space.

VF14 - Public Space								
	Enclosure Ratio	Width between buildings	Building line compliance	Parking	Cycling	Footway	Street Trees	Tree Spacing
High Street	Max 1:1.8 Min 1:2.7	16-20m	75%	One side of street	2m on both sides	2-3m	One side alternating	Min 12m Max 24m
Secondary Street	Max 1:1.5 Min 1:2.3	14-17m	65%	Both sides	On Carriageway	At least 2m	Both sides	Min 12m Max 24m
Tertiary Street	Max 1:1.4 Min 1:2.2	13-15m	65%	Both sides	On Carriageway	Part of shared surface	Both sides	Min 12m Max 24m
Parkside Street	NA	NA	75%	One side of street	On Carriageway	At least 2m	One side	Min 12m Max 24m

Village Fringe
High Street
Example Section



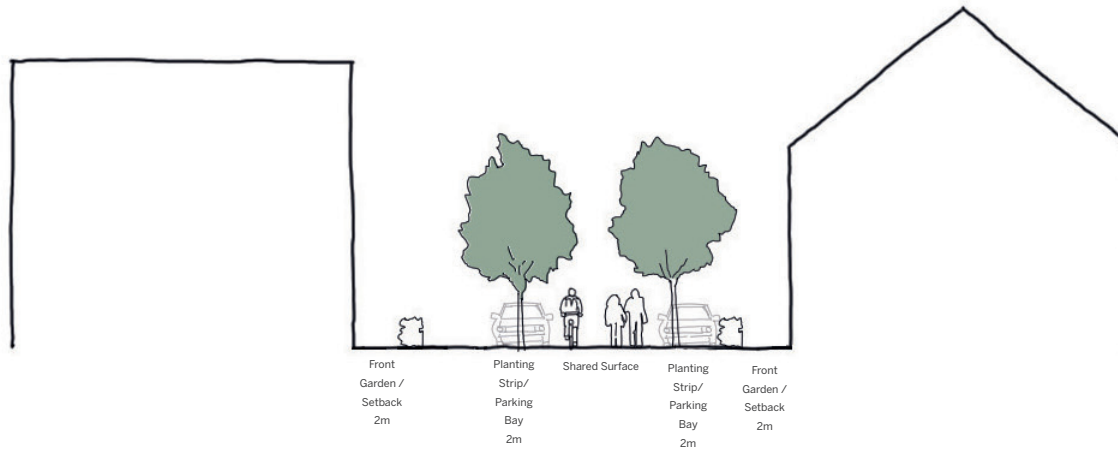
Horsted Park, gable ended frontages

Village Fringe
Secondary Street
Example Section



Old Lodge Farm - variation roof and articulation of front facade

Village Fringe
Tertiary Street
Example Section



Great, Knighton - tertiary street

3.2 Area Type 3: Rural Edge

Location

This area type covers the area to the east of the new village area and includes the northern and southern edges of the existing village.

The area includes high streets, secondary, tertiary and parkside streets, though the former will not include any shops.

Character

The character of the area is based on an analysis of nearby semi-rural settlements like St Mary's Hoo. The area is more rural in nature, and this is reflected in the following characteristics:

- Housing densities of between 15 and 25 dwellings per hectare.
- Predominant heights of 2 storeys.
- Buildings that are generally semi-detached, detached or bungalows.
- Buildings that informally follow a building line.
- Buildings that are set back 5-7m from the back of pavement.



Rural Edge Area Location

Rural Edge Coding

The following design codes set out the regulatory rules for the Rural Edge area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Rural Edge.

RE 1 - Streets:

Development in the village centre will define the Primary, secondary and tertiary streets shown on the plan on page 33.

RE2 - Cycle Parking:

To be provided based on 2 spaces per dwelling to be provided on-plot.

RE3 - Car Parking:

To be provided based on 2.5 spaces/residential unit. On average, 2 spaces be must provided on-plot with the remaining being unallocated on street parking. The on-plot parking can be in front of the building line, but care must

be taken to ensure that parked cars do not dominate the street scene.

RE4 – Green spaces:

The garden and courtyard spaces must be designed to maximise biodiversity. Garden space should occupy at least 60% of each plot. 60% of plots should have a minimum garden depth of 15m.

RE5 – Housing Density:

New housing must be built at net densities of 15-25 dwellings / hectare.

RE6 – Party Wall:

Party walls are not allowed.

RE7 - Form:

Development must create a varied and fine-grained development form with an informal layout and a range of materials.

RE8 - Building Line:

An informal building line must be set for development. This must be set 2-7m from the back of the pavement. Buildings must project forward and back from the building line by no more than 1.5m. Buildings should not come within 1.5m of a plot boundary.

RE9 - Height:

The maximum building height must be 2 storeys with an eaves height not exceeding 7m. The total height of buildings must be no more than 3m above the eaves height and most buildings will have pitched roofs.

RE10 - Boundary Treatment:

The boundary of front gardens should be marked by a hedge, timber fencing, cleft post or metal railings no more than 1m in height.

RE11 - Identity:

Buildings whether contemporary or traditional design should respect local vernacular:

- At least 70% of entrances must be marked with porches.
- Rooflines must be varied with some gables facing the street.
- A variety of materials must be used, which could include red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.

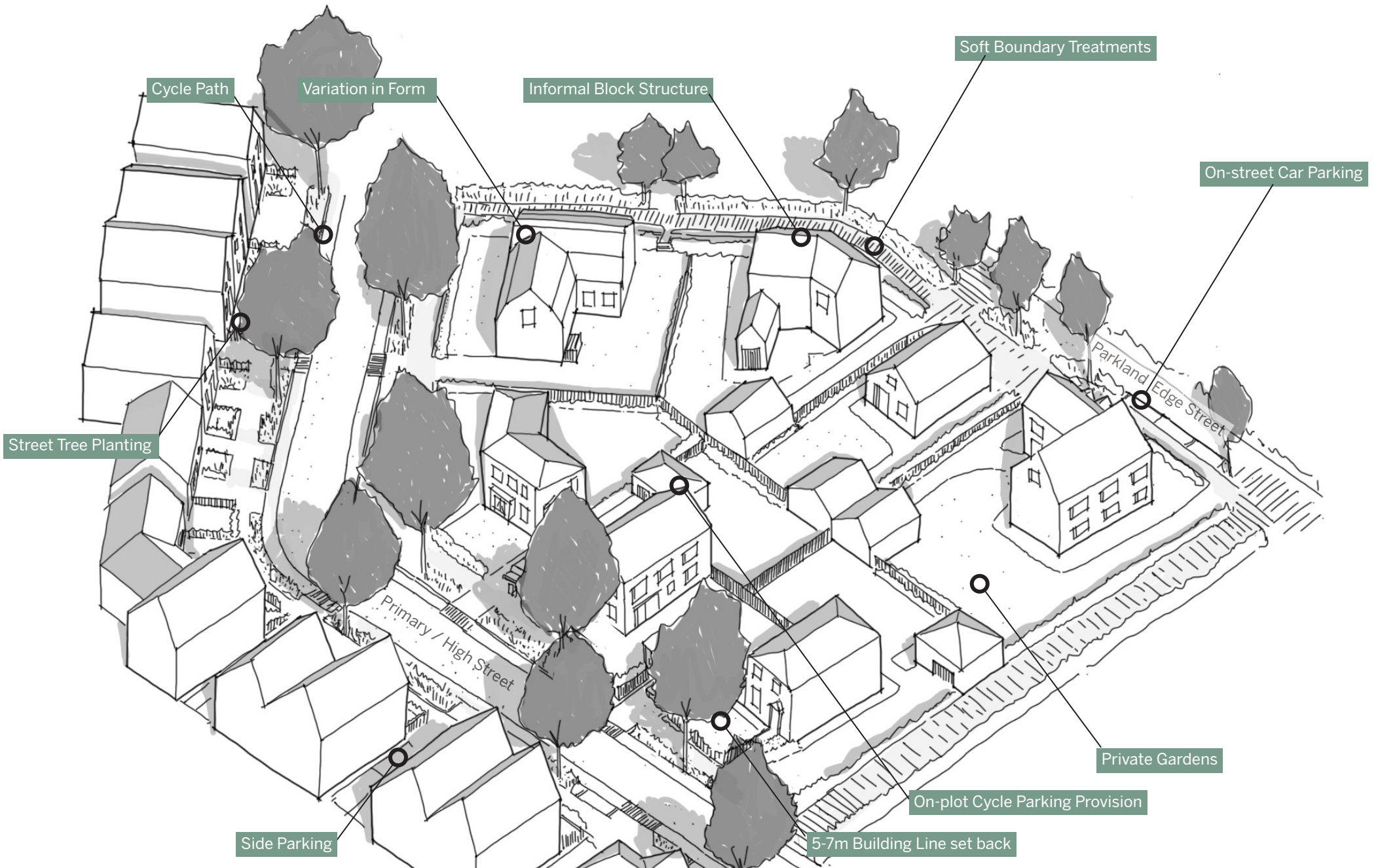
- Windows should be orientated vertically with the use of bay windows.

RE12 - Privacy distance:

A minimum privacy distance of 20m will apply between windows to the rear of properties.

RE13 - Wayfinding:

Development must provide a palette of bespoke wayfinding elements including public art and sensory elements within the public realm to reflect the distinctiveness of the rural edge area and enhance the experience of moving within it.



Rural Edge Area Character

RE14 - Public Space:

New public spaces must be designed to the specifications set out in the adjacent table.

RE15 - Mix of uses:

This area will be predominantly residential; however, development plots must provide enough rear amenity space for the option to accommodate a rear home-working studio whilst leaving a minimum area the size of the dwelling footprint for private amenity space.

RE14 - Public Space								
	Enclosure ratio	Width between buildings	Building line compliance	Parking	Cycling	Footway	Street Trees	Tree Spacing
High Streets	Max 1:3 Min 1:3.3	20-30m	60%	Occasional	2m on both sides	2m	One side alternating	Min 12m Max 24m
Secondary Street	Max 1:2.4 Min 1:2.8	17-24m	50%	Occasional	On Carriageway	At least 2m	Both sides	Min 12m Max 24m
Tertiary Street	Max 1:2 Min 1:2.5	15-20m	50%	Occasional	On Carriageway	Part of shared surface	One side alternating or both sides	Min 12m Max 24m
Parkside Street	NA	NA	60%	Occasional	On Carriageway	At least 2m	One side	Min 12m Max 24m



St Mary's Hoo - Gable-ended garages



St Mary's Hoo, detached housing, wide frontages short set-back with white picket fence



Hall Road, St Mary's Hoo - deep set backs and soft landscape boundary treatments

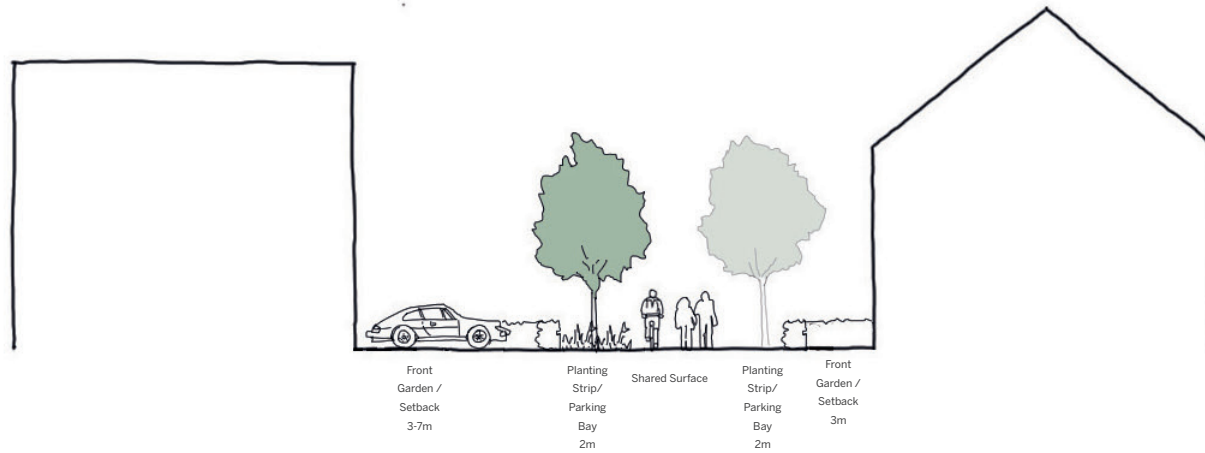
Rural Edge
High Street
Example Section



Rural Edge
Secondary Street
Example Section



Rural Edge
Tertiary Street
Example Section



3.3 Area Type 4: Rural

Location

The Rural Area type applies to the remaining land area within the Neighbourhood Plan boundary but excludes the existing built settlement area of High Halstow, development along Sharnal Street (A228) and the land east of High Halstow.

Character

The existing character of this area is also identified and described within the High Halstow Historical Area Assessment. The area is predominantly made up of rural marshland, farmland and woodland. The northern third of the area is an expanse of low-lying reclaimed marsh, bisected by fleets and ditches.

The upland parts contain farmland that is arable, orchards and woodland, including the nature reserve at Northward Hill, Wybornes Wood and Fishers Wood.

Several cottages and outbuildings were used by sheep minders in the 19th century, with the only remaining example called 'Shade House' located north of Decoy Fleet, an enigmatic brick structure.

Several dispersed upland farmsteads have survived as well as their farmhouses without their outbuildings. A group of four listed farmhouses in the south and west of the parish are located within the area.

The oldest of the four is Grade II listed Great Dalham on the Cooling Road, with a timber-framed structure clad with weatherboard. This building dates back to the early 16th century.

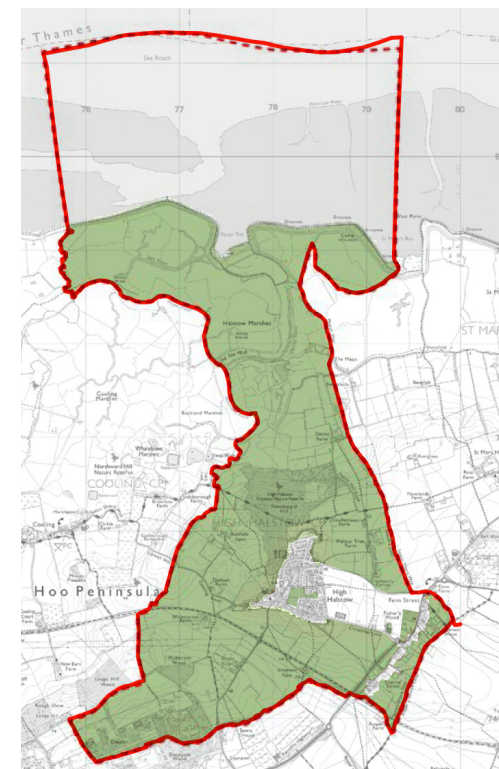
Dux Court, Wybourne Farm and Buckhole Farm are also Grade II listed red-brick farmhouses that range from the late 17th century to the early 19th century.

Other surviving buildings include Solomons Farm, south of the mineral railway, and Decoy Farm to the north of Clinch Street; the latter may have replaced a farmstead associated with the decoy pond in the marshes, lost its farmhouse in the 1960s.

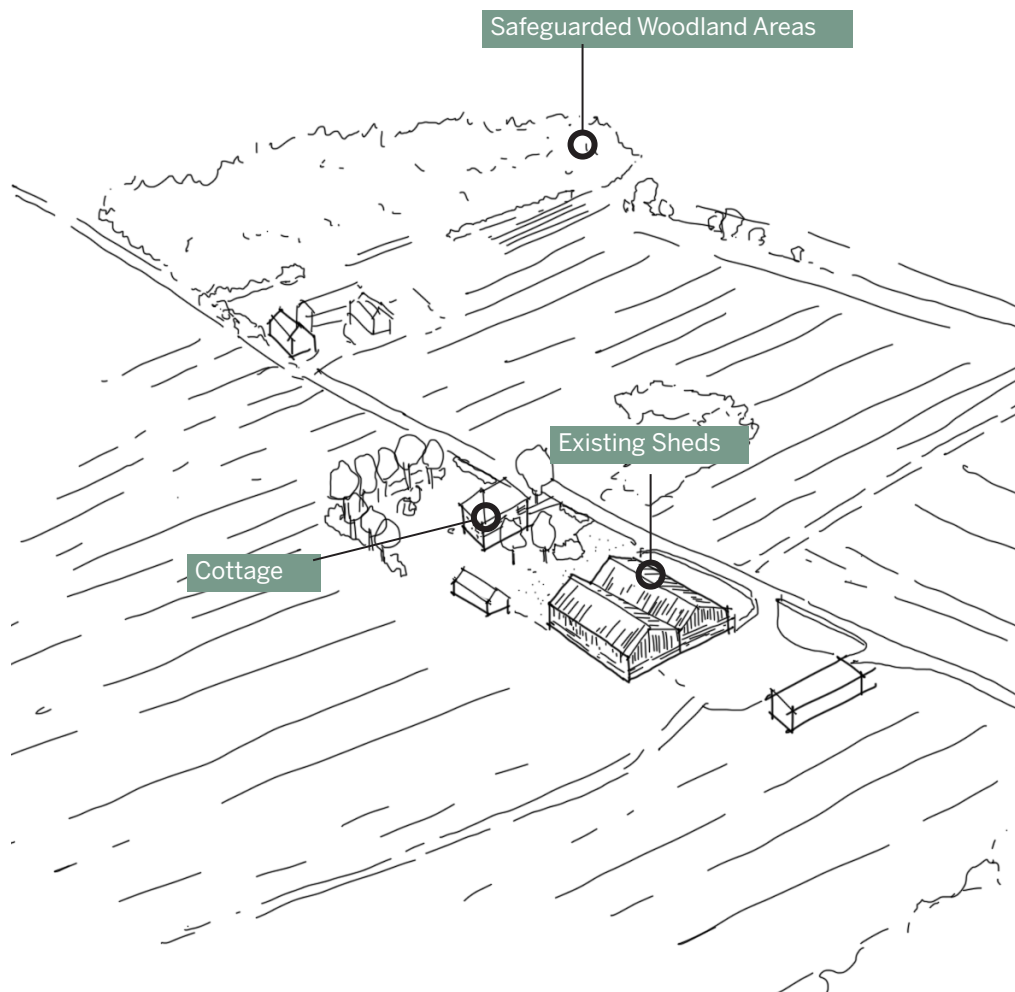
Two farms remain from the small cluster at Clinch Street, Clinchstreet Farm and Walnut Tree Farm.

Northward Hill is one of the higher points in the peninsular and is covered by a mixture of hawthorn scrub and ancient oak woodland.

This area has formed part of the High Halstow National Nature Reserve, managed by the Royal Society for the Protection of Birds. The nature reserve is an important educational and leisure asset for residents and visitors to the area.



Rural Area Type Location



Rural Area Type Character

Rural Coding

The following design codes set out the regulatory rules for the Rural area, which development proposals must adhere to.

R1 – Green spaces:

Any development proposals that would cause unacceptable harm to the local landscape character, archaeological or nature conservation will not be permitted; proposals must be evidenced with a LVIA, TVIA, conservation appraisal and archeological appraisal and development must be in line with the latest policy requirements and best practice.

R2 - Identity:

Any proposed extension or redevelopment must be designed as a contemporary or traditional interpretation of local vernacular:

- Rooflines must be varied with some gables facing the street.
- A variety of materials must be used and could include red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.



Decoy Farm - Sheds and Cottage



Walnut Tree Farm on Clinch Street



Shade House, High Halstow Marshes

Appendix

*This section provides information on the design
coding process*

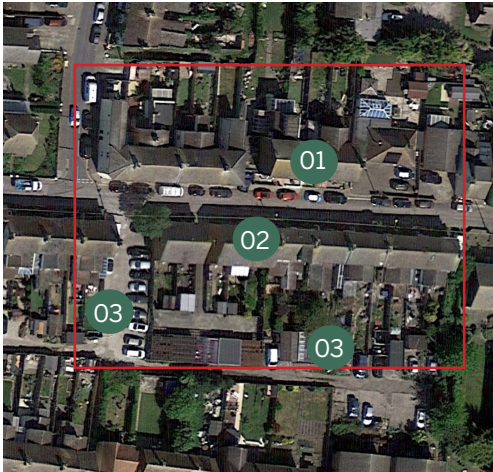
04

4.1 Design Code Precedents

Village Centre Area Type Precedents

The villages of Cliffe and Lenham in Kent have inspired the character of the village centre area type. In particular, the village centre area type takes inspiration from the character of Reed Street in Cliffe and the High Street and Village Square in Lenham. The features identified in both precedents are set out in a series of tables and images.

Village Centre Precedent - Reed Street, Cliffe Local Character	
Feature	Measure / Description
Block type	55-60m deep residential development block with rear gardens and courtyard parking to the rear and side.
Density	55dph
Building heights	2-3 storey
Width Between Buildings	8m - 14.5m
Street Enclosure	Between 1:1.6 and 1:0.8
Setbacks	0m - 2m with the occasional larger 6m setback from the building line
Boundary Treatment	None, low brick wall, low brick wall and railing
Parking	On-street, rear courtyard parking and side courtyard parking and rear garages.
Back to Back Distances	35m - 40m
Rear Garden Depth	10m
Building Identity	Duo pitched gable roofs Variation in roofline Pitched roofs Occasional recessed or protruding porches Vertical bay windows Red / yellow / buff brick walls Weatherboard



1. The terrace provides a side courtyard parking area and rear parking areas with rear courtyard parking areas. From the google map, it shows these spaces to be used. Additional parking is noted on-street, though no bays have specifically been provided.



2. Long storey terraces with variation in building facade materials ranging from white weatherboard, yellow/buff brick walls, and protruding porch and garden frontage low brick walls.



3. The terrace provides a side courtyard parking and rear parking areas with rear courtyard parking areas. From the google map, it shows these spaces to be used. Additional parking is noted on-street, though no bays have specifically been provided.

Village Centre Precedent - Village Square, Lenham Local Character	
Feature	Measure / Description
Block type	Continuous terrace block with active frontages that frame a historical village square. The village square is dominated by car parking, however, it transformed into space for the village market. Buildings turn the corners at the edges of the village square.
Density	55dph
Building Heights	2-3 storey
Width Between Buildings	12.5m
Street Enclosure	1:2.1
Setbacks	0m - 0.5m privacy threshold
Boundary Treatment	None, differing ground material treatment
Parking	On-street parking bays, rear courtyard parking and side courtyard parking and rear garages.
Back to Back Distances	17m-20m
Rear Garden Depth	7m-13m
Building Identity	Terraces with gable fronts and duo pitched gable roofs Variation in roofline Occasional recessed or protruding porches Rooftop dormers Pitched roofs Vertical bay windows Red /yellow brick walls, painted, mock tudor Weatherboard Off-white rendered walls



1. Village square framed by the surrounding buildings on the village market day.



2. Lenham High Street with active frontages, 2-2.5 storey pitch roof buildings with a variation in the roofline.

Village Fringe Area Type Precedent

Areas within High Halstow have inspired the character of the village fringe area type. The features identified in both precedents are set out in a series of tables and images.

Village Fringe Precedent - High Halstow Local Character	
Feature	Measure / Description
Block type	60m deep residential development block with rear gardens and courtyard parking to the rear and side.
Density	30-40dph
Building heights	1.5-2.5 storey
Width Between Buildings	16m-30m
Street Enclosure	Between 1:2.5 and 1:3.3
Setbacks	Generally between 3m-6m with some variation.
Boundary Treatment	Green boundaries and mature landscaping / white picket fence / low brick wall and hedge
Parking	On-street, rear courtyard parking and side courtyard parking and rear garages.
Back to back distances	40m-45m
Rear garden depth	7m - 10m
Building identity	Duo pitched gable roofs, hipped roofs Variation in roofline Slate grey roof tiles Occasional recessed or protruding porches Vertical bay windows Red / yellow / buff brick walls, weatherboard, tile hanging



High Halstow Village, Aerial Map



1. Cornish style semi-detached units, with a 6m setback from the pavement with side parking drives and garages. Buildings are set-backs from the edge of the pavement vary between 3-6m.



2. 2-2.5 storey yellow brick flats and terrace with a hip clay tile roof and varying roofline. The plot has a rear parking courtyard with garages for the residents of the flats and terrace. Hedges and a white picket fence demarcate the plot boundary.



3. 2-2.5 storey red and yellow brick terrace with gable-ended/pitched roofs, pitched roof porches, and dormers. A white picket fence and hedges mark the plot boundary.



4. 2 storeys, detached property comprised of ground floor light red brick walls and first-floor weatherboard cladding, vertical bay window feature. The buildings are set back from the edge of the pavement by 6m and provide a side parking drive and integral garage.

Rural Edge Area Type Precedent

Areas within St. Mary's Hoo and High Halstow have inspired the character of the rural edge area type. In particular, the village centre area type takes inspiration from the character of Hall Street in St. Mary Hoo and Christmas Lane in High Halstow. The features identified in both precedents are set out in a series of tables and images.

Village Fringe Precedent - St. Mary Hoo Local Character	
Feature	Measure / Description
Block type	35m deep residential development half block with courtyard parking to the rear and side parking.
Density	20dph
Building heights	1.5-2 storey
Width Between Buildings	10m - 20m
Street Enclosure	1:2
Setbacks	Generally between 1m-8m
Boundary Treatment	Green boundaries and mature landscaping / white picket fence / low brick wall and hedge
Parking	Rear courtyard parking, side courtyard parking and rear garages.
Back to back distances	40m-45m
Rear garden depth	14m - 20m
Building identity	Duo pitched gable roofs, hipped roofs Variation in roofline Slate grey roof tiles / brown clay tiles Occasional recessed or protruding porches Vertical bay windows Red / yellow / buff brick walls, weatherboard, tile hanging



Hall Lane, St. Mary Hoo Aerial Map



1. Semi-detached property with yellow, buff brick walls, clay tile roof and pitched dormers.



2. St Mary's Hoo - Gable-ended side garages with driveways.



3. Detached housing, wide frontages short set-back from the edge of the street with a clay tiled hip roof.



4. St Mary's Hoo, detached housing, wide frontages, short set-back with a white picket fence.



Christmas Lane, High Halstow Village
Aerial Map



5. Christmas Lane, two-storey housing pitched roofs with deep front gardens and soft landscape boundary treatments.

Rural Edge Precedent - Christmas Lane, High Halstow Local Character	
Feature	Measure / Description
Block type	50m deep residential development block
Density	10dph
Building heights	1-2 storey
Width Between Buildings	15m - 25m
Street Enclosure	Between 1:3 and 1:5
Setbacks	Generally between 6m - 25m.
Boundary Treatment	Green boundaries and mature landscaping / white picket fence / low brick wall and hedge
Parking	Integral garages and side parking
Back to back distances	20m - 25m
Rear garden depth	20m - 60m
Building identity	Duo pitched gable roofs, hipped roofs Variation in roofline Brown clay roof tiles Occasional recessed or protruding porches Vertical bay windows Red / yellow / buff brick walls, weatherboard, tile hanging.

4.2 Consultation Process

Parish Council & Landowner Engagements

The Design Code and the design coding have been developed over a year through ten meetings between a core group of ten residents and elected members of the Parish Council and URBED.

As part of the co-design process, the Parish Council and URBED engaged with the Senior Urban Design Officer at Medway Council, who provided additional advice and feedback on the Design Code.

The Parish Council and URBED organised meetings with the landowners Redrow PLC and Dean Lewis Estates Ltd. to explore and agree on a range of design principles for the Land East of High Halstow if it were to come forward as a site allocation within the Local Plan.

The meetings took place online on Zoom between May 2021 and April 2022 (due to COVID-19 restrictions) and fed into the development of the Draft Regulation 14. Design Code and the final version of the Design Code.

Parish Wider Community Consultation

The High Halstow Parish Council held a wider community consultation event in person at the High Halstow Village Hall on 12th September, 2021.

The purpose of the consultation event was to share the Neighbourhood Plan proposals and to get initial feedback on three masterplan options for the Land East of High Halstow that would form part of the Design Code.

The feedback from the consultation identified Option 2, showing access to the site from the roundabout connecting to Ratcliffe Highway, as the preferred option.

This option was included as the main option in the follow-up Reg. 14 Design Code Consultation.

High Halstow Neighbourhood Plan Regulation 14. Design Code Consultation

The High Halstow Neighbourhood Plan regulation 14 Design Code consultation ran from 31st October 2021 to 14th February 2022.

Reg. 14 Draft Design Code was published and shared online and in-person with statutory consultees and the wider community on 27th October.

Responses and comments on the draft Design Code were collected online via a website form and physically via a paper consultation form sent to all households. A total of 99 responses were received, along with a further 603 resident comments.

Written responses were received from Medway Council, Dean Lewis Estates Ltd. Homes England, The Environmental Agency and Redrow PLC.

A full table summarises this consultation's specific comments on the Reg. 14 Design Code, and the updated Design Code response is set out within section 4.3.

Design Options presented at the Wider Community Consultation and the High Halstow Neighbourhood Plan Regulation 14. Design Code Consultation



Option 1



Option 2 (preferred option)

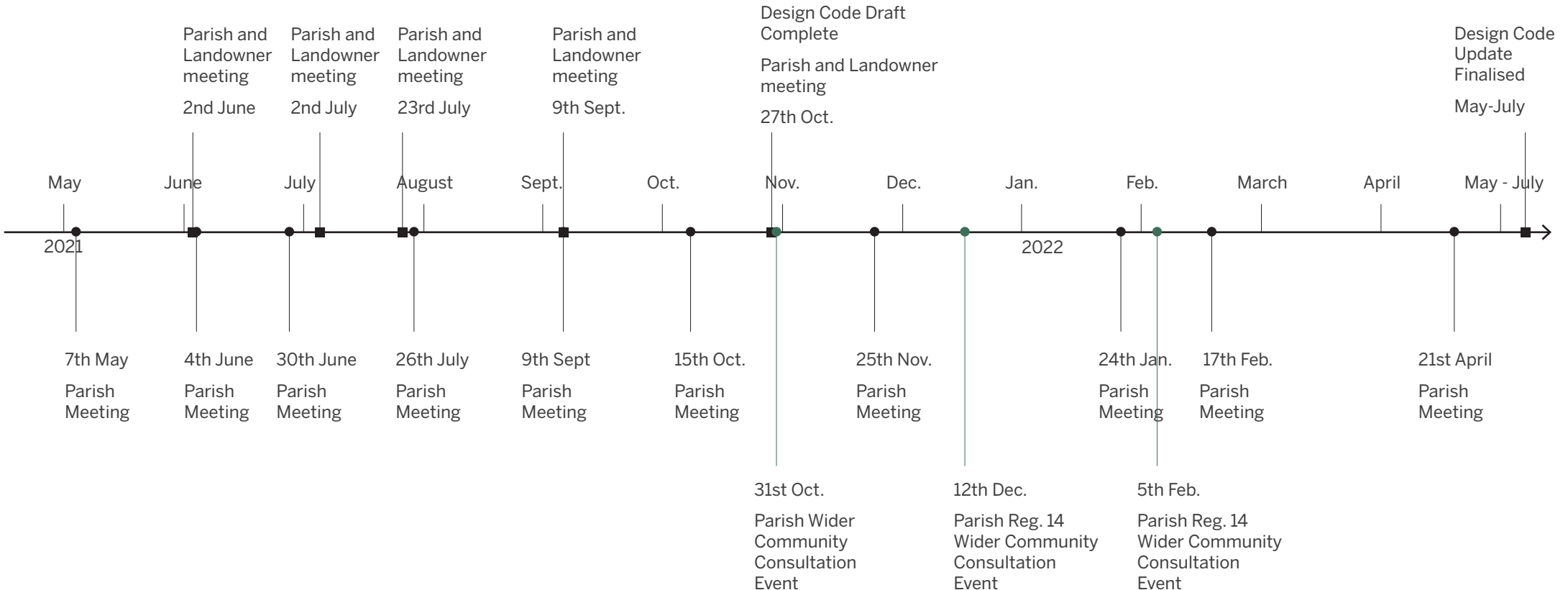


Option 3

Parish Council & Landowner Engagement

Reg. 14 Design Code Consultation

Feedback & Response Collection



Design Coding Process Timeline

4.3 Reg. 14 Consultation Responses

Summary of Community Comments & Responses		
Topic	Comment	Design Code Update Response
Development should not go ahead.	Development of the site should not go ahead.	The Design Code recommends that the site should not be developed. If the site, however, is designated as an allocation within the Local Plan and infrastructure measures are put in place for the site, the Design Code supports the Neighbourhood Plan in ensuring that the development of the site will adhere to best practice design standards.
New school site on the Land East of High Halstow	The community would welcome the old school site as a new centre but there is a concern about parking for any new centre that includes a new school site.	The Design Code has been updated to provide a code about parking where there will be no stopping for the school or no parking in the village square. The design code provides a code for designated parking for the school, within the school site area.
Density	Density looks too high in the Village Centre and density across the new development looks too high.	The Village Centre Area type covers only a small part of the development area, and the higher density band provides a distinction in character from the other area types. The density is not too dissimilar from the density found within the centre of other villages in Kent, such as Cliffe and Lenham, which help to frame a village square space for activities such as market days.
Distances between housing	Generally houses are being built too close together and the density is too high for a village environment.	The Design Code provides a minimum garden area of 60% of the plot which will reduce instances where housing is built too close together. The rural area type density will also be reduced to 15-25dph from 25-35dph to address density concerns and to emphasise the rural edge character.
Existing School Site	There are concerns around the future use of the existing school site.	The Design Code itself cannot determine what future will be provided on the existing school site as this is determined by whoever owns the land in the future. The Council, though, recommends that this land is provided for mixed-use community / residential uses, but this is yet to be determined.
Provision of a new primary all vehicle access from the roundabout on Ratcliffe Way	Comments and feedback from HH Parish and wider community consultation agree with preferred access point - 98% agreed with the Parish Option as opposed to the Redrow Option.	The Design Code will keep the vehicle access from the roundabout on Ratcliffe Way as the preferred access point into the site as 98% of residents voted in favour of this option.
Village Centre Parking and Use of parking courts	There is concern that there will not be enough parking of rear parking courts that they will be underutilised which will cause flyparking in the village centre area.	The typology for rear courtyards appears only within the higher density village centre areas, covering only a small site area. The design code recommends on-plot rear parking, side parking solutions, or on-street car parking allocated bays for the village edge areas. The code will add a communal car parking area in the village centre area for visitors to address other parking needs within the village centre, including electric car charging points. An example precedent is the village of Faversham.

Topic	Comment	Design Code Update Response
Landscaping, green spaces, drainage features, tree placement and footway width coding.	95% strongly agreed with No1-4 and felt sewage was covered in the Draft design code and agreement	The code references the NMDC guidance best practice principles that future development must adhere to in the design of landscape & green spaces and drainage features. Tree placement and footway width recommendations are already provided within each area type but maintain some flexibility for creative interpretation.
Solar panel and electric generation	R01 - there is no actual mention of solar panel heating and electric generation.	R01 is an umbrella code for sustainability measures that will include a number of electric generation and solar heating systems to ensure the homes will be net-zero ready. The design code will ensure that new houses will be zero carbon ready.
Rainwater Storage Provision and Use	R05 no mention of rain water storage and use	R05 is an umbrella code for a sustainability measure to conserve water; solutions to meet this target will have to consider a number of water conservation strategies, including rainwater storage and use.
EV Charging Points	Scheme should include electric car charging points.	The Design Code will include a code to provide electric vehicle charging points within homes with on-plot parking spaces, on-street parking bays and within the communal parking area.
Closing of Chrstimas Lane (to vehicles)	Amongst the responses to the Design Code ...2 were in favour of closing, and eight were against. Those against the closing are not against providing safe pedestrian and cycling but suggest that any development that doubles the size of the village is unsustainable without a significant additional road. In that context closing, any road or part of a road makes no sense. Some respondents suggested widening Xmas Lane in order to provide safe pedestrian and cycling.	The Design Code will maintain that the closing of Christmas Lane is a potential move, but in any instance, if closed would still provide access for emergency vehicles, pedestrians and cyclists.
Housing for young families	The design code could add a note about including housing for young families.	The updated design code provides a point on including housing for young families in HB01 on p15.
School parking	The desing code could provide a point to include parking within the school block.	The updated design code provides a point on including parking for the proposed school and its ancillary staff on p16.
Allotments	Allotment standards could be incorporated within the design code.	The design code has been updated to include code N07 which sets out allotment provision for the Land East of High Halstow.
Non-domestic buildings	The design code could increase the energy efficiency standards from excellent to outstanding.	This has been accepted and updated in code R04 on p15.
Provision of a mini supermarket and post office	The design code could be more explicit about including a mini supermarket and a post office.	This has been accepted and updated in code U03 on p14.
Permeable paving	Permeable paving could be included as part of the design code.	This has been accepted and updated in code N04 on p13.
Bins for flats	Space for bins in flats could be included within the design code.	This has been updated within code M07 on p13.
Rainwater recycling	The design code could include more details on rainwater recycling.	Rainwater butts for rain water collection has been added in HB04 - Gardens on p15.
Solar panels	The design code could include more details on solar panels.	The design code R01 includes a note on solar panels on p15.
Green roofs	The design code could include more details on green roofs.	The design code N05 includes a note on green roofs to increase biodiversity on p15.

Summary of Medway Council Comments & Responses

Topic	Comment	Design Code Update Response
Density & Plot Coverage	Need to ensure that the density reflects net densities - could use plot coverage figure to meet local expectations more appropriately. Separately, Medway's current design guidance expects a minimum privacy distance of 20 metres, although this is likely to vary across different types of area, and could be similarly varied within your area codes through discussion and where demonstrated.	The Design Code will provide a minimum garden area of 60% of the plot within the Rural Edge area. The design code will also specify that party walls are not allowed and that buildings should not come within 1.5m of a plot boundary, which will reduce instances where housing is built too close together. The rural area type density will also be reduced to 15-25dph from 25-35dph to address density concerns and to emphasise the rural edge character. The design code will provide a code to set minimum distances between back to backs of housing, set at 17m for housing within the Village Centre, 20m for housing within the Village Fringe, and 20m for housing within the Rural Edge.
Homeworking	Need clarification of sheds as home working spaces.	The design code will remove indication of sheds as being for home working purposes within the visual.
Design Code Enforcement of design coding language	Need stronger language and minimum targets throughout the document.	Design Code will replace 'should' to 'must' across the document, though we would argue that there are minimum targets already set within the document.
Design coding for redevelopment of existing buildings.	Need clarification around where redevelopment of existing buildings for example individual homes or plots would fall under the code.	The updated Design Code will be updated to cover the existing village area, and any redevelopment of existing buildings will need to follow the new coding set out in the design code.
Include a minimum on-plot parking in front of the building line	Coding around on-plot parking in front of the building line could benefit from a minimum criteria.	From further discussions with Medway Council Principal Urban Design Officer, no change would be needed. The maximum means the majority of the on-plot parking provision must be delivered behind the building line, either to the side of the homes or in rear courtyards, to discourage parking directly in front of homes.
Self-build plot coding	The code does not consider the nature and requirements for self build plots given the policy requirements set out in the draft plan itself. The coding could determine the distinctiveness and variety within character areas and existing village based on thier design and placement and how the plots could be integrated into other development	The updated Design Code refers to the design guidance on self-built plots set out within the neighbourhood plan.

Topic	Comment	Design Code Update Response
Landscaping, green spaces, drainage features, tree placement and footway width coding	The code should go further in its consideration of the design of landscapes, green spaces and drainage features, including tree placement and footway width, which could be used to help further differentiate each character area within the code.	The code references the NMDC guidance best practice principles that future development must adhere to in the design of landscape & green spaces and drainage features. Tree placement and footway width recommendations are already provided within each area type but maintain some flexibility for creative interpretation.
Public art & Wayfinding	The code could add coding around art and wayfinding	The updated design provides Public Art & Wayfinding coding.
Additional caveat on additional community consultation for 50+ units	The design code could provide an additional caveat on additional community consultation for 50+ units and an addition could highlight that the Parish Council is already a statutory consultee on planning applications	The updated design code accepts the text addition.
Village centre coding flexibility	Within the Village Centre code flexibility could be written into VC6 allowing for exceptions for elevations facing intersecting streets to clarify the calculation of some of the figures.	The updated design code accepts the text addition.
Corner Plots	Corner plots are mentioned earlier in the document and labelled in the diagram for the village centre area but appear to lack any clear coding direction which could be rectified.	The updated design code will add to VC6 that buildings should turn corners.
Design Code plans update	The maps throughout the code need to show both new and existing areas of the village in that area.	This has been accepted and plans have been amended.
Rural Code R1	There are concerns as to how 'unacceptable harm' will be interpreted and how this is linked in with the policies in the plan.	Accept updates - Text references the tests LVIA, TVIA – conservation appraisals and archeological appraisals, and in latest policy requirements and best practice.

Summary of Landowner Comments & Responses

Topic	Comment	Design Code Update Response
Community Engagement	How were the views of the wider population captured in developing a vision for each Area type? - 5.18 'no information or evidence of how the views of the wider community have been captured at any of the stages of the development of the design code. If this is not possible for whatever reason the DC should clearly explain why the recommended process of involving the wider community at each stage of the development of the design code has not been followed. The introductory sections should also include reference to the design workshops held with the developers and the outputs from each.	The NMDC process sets out a suggested design process for Local Authorities to develop Design Codes. The consultation process with the Parish Council - a group of residents of the local area. The group have engaged in a series of workshops exploring various options for the design and feedback on the character they would want to see in the development of the land east of High Halstow. The wider community has been consulted by the Parish Council within the Reg. 14. consultation brought back 600+ comments and feedback and general agreement on the coding. This feedback has been taken into consideration by the Parish and URBED. The Design Code update will provide a summary of the consultation process.
Local Character Assessment	The DC should also include a detailed assessment of local character and reference densities, street cross-sections, building setbacks as well as predominant housing types, boundary treatments and parking arrangements. This assessment should focus on High Halstow but also reference other local villages. Without this level of assessment, it is difficult to see how the contents of the design code will provide for a responsive development.	In initial discussions with the Parish, we discussed the characteristics of a number of village precedents in Kent. They were omitted as part of the main document for ease of legibility and use for future planning applications but will be appended.
Movement M01 Streets Street	New streets should provide for good connectivity by all modes and encourage walking and cycling. Cul-de-sac and private drives can form part of the street network as tertiary level streets.	Disagree - The NMDC does not support the use of cul-de-sacs for quieter streets; it says that they are acceptable only as tertiary streets, though new streets should link at either end to other streets where possible. Cul-de-sacs do not enhance pedestrian connectivity; therefore the High Halstow Design Code does not see them as appropriate for development on the Land East of High Halstow.
Movement M06 Parking	The text as drafted precludes parking to the front of homes on any of the primary ('high street') or secondary streets. Instead the codes suggest parking is either located to the rear of homes in their rear gardens or within rear courtyards. This type of parking arrangement is inconvenient, impractical and would lead to issues such as pavement parking and conflicts with pedestrians and cyclists using the street.	M06 Parking - there is the option of providing parking within on-street bays and also along the side of properties as well as the rear courtyard parking areas.

Topic	Comment	Design Code Update Response
Nature N06 Street Trees	It is proposed that the text is revised to allow for the provision of good quality trees within front gardens to contribute to a tree-lined street.	Disagree - Planting in the front gardens reduces the onus on the developer to provide and maintain trees within the public realm. Tree planting should be provided within the public realm to improve the quality of the streetscape and public realm.
Identity I03 Identity of Buildings	The parish council's design code requires compliance with para 1.2 of the NMDC Guidance Notes. This guidance is too prescriptive and refers to the use of vertical window configurations which would not be reflective of the local architectural character of High Halstow.	Disagree - 1.2 of the NMDC Guidance Notes are not prescriptive but rather are key building design principles. They are not prescriptive of vertical window configurations - the illustrations only demonstrate the option of providing vertical window configurations across different examples. Note that the NMDC talks about the proportion of windows rather than whether they should be horizontal or vertical. 'Windows: Elevations work best with a wall to window ratio of 15-35% whilst taking into account of requirements to avoid overheating. The proportion and design of windows can shape the facade based on whether they are horizontal or vertical, and the depth of the reveals.'
Public Square PO3 Home Zones	This code requires that all tertiary level streets be designed to Home Zone principles. This is overly prescriptive and whilst some shared surface streets will be appropriate as part of the street hierarchy the text should be revised to allow greater flexibility.	Disagree - the code does not prescribe shared surfaces only as they can include low kerbs and separately marked pavements. There is flexibility in how the principles are applied from section P.2.iii of the NMDC Guidance.
Use U03 Shops	This code requires the delivery of at least five shop units and that they are let on terms that encourage occupation by local businesses. The delivery of a viable local centre needs to be based on commercial parameters and greater flexibility is required.	Updated - the wording has been updated to remove the specific number of shop units.
Use 05 Homeworking	The objective of homeworking is supported, but greater flexibility is required.	Disagree - it would be good to include space within the design for a homeworking hub which the developer has recommended omitting from this. We think this should be kept in as a requirement.
Homes HB01 Housing Design	It is suggested the code is revised to ensure all homes are well-designed, convenient and practical for the residents - including a cause about parking will be conveniently and practically located close to the front door.	Disagree - we want to reduce car dependency within the future development and encourage the use of more sustainable forms of transport and active travel within the village.

Topic	Comment	Design Code Update Response
Resource Coding R01 - R05	<p>The objective to help facilitate a sustainable pattern of development is supported and sustainable pattern of development that encourages walking and cycling will be delivered together with energy-efficient homes. However, the codes in R01 to R05 go beyond what is required by national policy or the Building Regulations and would affect the viability of the development. The government has set out a timeline for delivering zero carbon ready homes including the 2025 Future Homes Standard. Developers of this site would be required to meet the regulations in place and for this reason codes that repeat this are not necessary. For this reason, it is suggested that R01 – R05 are replaced with a single code.</p>	Disagree - The Design Code expects that new development in the area to be zero carbon.
Closing of Christmas Lane to Vehicles (Except for emergency vehicle access)	<p>The suggestion of closing part of Christmas Lane is not supported without further detail of what is proposed, the reasons it is proposed and evidence of support of the wider community for such a fundamental revision to access into and out of the village.</p>	Disagree - The closing of Christmas Lane to vehicles is a potential option within the Design Code, and further detail of the proposal would need to be considered.
Village Centre Area	<p>The extent of the proposed Village Centre Area Type shown in the plan is larger than is necessary and shows areas facing onto the green edge.</p>	Disagree - The higher density of the Village Centre area type defines the gateway and entrance into the village centre area.
Parking shown within the Village Centre Illustration	<p>The amount of parking indicated for the homes shown is significantly less than what would be required to meet the Local Plan standard. All of the allocated parking would be within rear gardens in this drawing and there are no parking areas other than these shown or indicated. There are approximately 60 dwellings shown in the central development parcel but only approximately 20 parking spaces (1 space per within each of the 20 rear gardens shown). This ratio of 0.3 spaces per dwelling would be completely insufficient and would result in parked cars in areas not designed to accommodate parking including pavements, grass verges, shared surface areas. This would create conflicts with pedestrians and cyclists and well as creating an inconvenient and impractical way of living.</p>	Disagree - Parking in this area can be accommodated within on-street parking bays as well as in rear courtyards, and parking bays are not required to be provided on-plot. An update to the design code will be made to provide a dedicated car parking area for additional visitor parking and will be increased to 50% within the coding to accommodate additional parking in the village centre for visitors.

Topic	Comment	Design Code Update Response
Rear Garden Car Parking	The use of rear gardens for car parking is also not realistic, convenient or appropriate. It is likely to result in fly-parking closer to the front door of the home.	Disagree - we would want to discourage future residents from using cars often to reduce overall traffic. Providing car parking in the rear of the gardens creates the option for residents to either use the parking space or for additional private rear amenity space.
Rear Parking Courtyards	Rear parking courtyards also result in insecure rear boundaries as well as reducing the use of front door which reduces the amount of activity at the front	Disagree- as this assumes residents will be leaving the home by car. Whilst parking is provided mainly in the rear for cars, people can exit the house through the front door.
VC2 Cycle Parking & VC3 Car Parking	VC01 and VC02 set out requirements for cycle and car parking that are different to those in the Local Plan. For the avoidance of confusion and to ensure that sufficient spaces are provided it is suggested that the text for each is revised.	Disagree - There are no requirements for cycle parking within the Local Plan so we would want to keep this in to enable a culture of cycling. Local Plan parking standards are out of date and old, if the village plans to move towards net-zero targets the village must reduce car use in the area.
VC4 Landscaping	As drafted VC4 is contradictory as it requires the design of the most urban part of the site to maximise biodiversity. The wider development will provide numerous areas for biodiversity rich landscape but in the Village Centre this would be challenging without compromising the quality and /or design of the space. Equally the school grounds have a number of functional requirements that may limit the extent to which biodiversity rich planting can be delivered.	Disagree - there are opportunities to introduce high-quality trees and planting in the new square and school grounds to maximise biodiversity in these spaces.
VC6 Party Wall	Revised to allow for a reasonable proportion of semi-detached housing types.	Disagree - the core of many historic village centres are made up of terraces.
VC8 Building Line	A slight variation to the setback distance would allow for some front gardens and greenery within the Village Centre Area.	Disagree - it is meant to be urban in character to create a more distinctive character to the area like other village centre areas

Topic	Comment	Design Code Update Response
VC9 Height	As drafted, this is unnecessarily confusing and it is suggested that simpler wording would be helpful to achieve the same objective.	Disagree - there is clarity on this.
VC10 Boundary Treatment	The character assessment has identified white picket fencing is a local characteristic boundary treatment in village centres such as Cliffe as well as dwarf walls.	Accepted - white picket fencing has been added to this code.
VC 11 Identity	It is not considered appropriate to prescribe that the architectural design approach should be 'a contemporary interpretation of local vernacular' especially in the absence of any evidence for why this is the best design approach and/or any evidence that this is the preference of the wider community. Slate and clay tiles are very expensive, and a high-quality architectural treatment can be achieved without using natural materials for roof coverings.	The wording has been updated for more flexibility on new development to respect local vernacular.
VC13 Mix of Uses	The number, size and detailed use of retail units will be informed by commercial market assessments.	Accepted - the specified number of the retail units has been removed to allow for flexibility.
VC 14 School	The design and position of the school building will be informed by a wide range of technical requirements. It is therefore necessary to allow for some flexibility in this respect and the text for this code.	Diagree - these are design coding principles that will enable to school to best frame and relate to the village square.

Topic	Comment	Design Code Update Response
VF1 & 2 Cycle Parking	VF 1 and VF 2 set out requirements for cycle and car parking that are different to those in the Local Plan. For the avoidance of confusion and to ensure that sufficient spaces are provided it is suggested that the text for each is revised.	Disagree - There are no requirements for cycle parking within the Local Plan so we would want to keep this in to enable a culture of cycling.
VF4 Green Spaces	The principle of optimising greenery and landscaping within streets is welcomed. However, further clarification and specification is required to reflect the particular way that this is achieved in High Halstow streets.	Accepted - we will add additional coding for front/back gardens and landscaping within the streets (rain gardens, shrub/tree planting on street verges).
VF5 Housing Density	"The average density of existing High Halstow is approximately 17dph although it ranges from 6dph to 33dph. Whilst it is acknowledged that a more efficient use of land is appropriate on the land to the east of the village the proposed density of 35-45 is considered too high and will not enable the creation of streets with distinctive 'open character' of High Halstow streets described in the character assessment of the village in the Hoo Development Framework. The density code for this Area Type should therefore be revised."	Accepted - to reduce the density in this area type to 30-40dph.
VF6 Party Wall	This code requires that every home should be attached to another on at least one side. This means that every home in this Area Type would be either semi-detached or terraced. This would by default generate a net density of over 50dph (outside of the prescribed range for this Area Type) as well as result in a character and density that would be alien to High Halstow which is comprised of 60% detached homes and a density range of 6dph-33dph. Instead, VF 6 should be revised to allow for a similar proportion of detached homes to High Halstow.	Accepted - the Design Code provides additional detail on a range percentage of at least 80% of housing should be joined to their neighbourhood on at least one side, creating a party wall which means 20% of housing can be detached.

Topic	Comment	Design Code Update Response
VF7 Form	The wording of this code, requiring a 'fine grained development form' with 'narrow plots' is not at all reflective of High Halstow and would generate a place with a character that is alien to that of the existing village.	Disagree - this creates variety in the character of the streets.
VF8 Building Line	This code requires revision to allow for homes to be set back behind landscaped boundaries reflecting the streets of High Halstow.	Disagree. There is a 2-5m set-back which allows for landscaping in front of homes.
VF9 Height	Required for clarity.	Disagree - there is clarity on this.
VF10 Boundary Treatment	The wording of this code should be revised to allow for the use of the full range of locally distinctive boundary treatments in this area.	Accept this - The boundary of front gardens should be defined by a hedge or white picket fencing. Cleft post and rail timber fencing to open spaces.
VF11 Identity	It is not considered appropriate to prescribe that the architectural design approach should be 'a contemporary interpretation of local vernacular' especially in the absence of any evidence for why this is the best design approach and/or any evidence that this is the preference of the wider community. Slate and clay tiles are very expensive and a high quality architectural treatment can be achieved without using natural materials for roof coverings. There is no evidence for why 'vertical windows' are appropriate here as this is not a characteristic of High Halstow.	The wording has been updated for more flexibility on new development to respect local vernacular.
VF13 Mix of Uses	"Revised to highlight the benefits of detached homes for homeworking."	Updated - the Design Code has removed home-working element and include space within all homes for home-working opportunities.

Topic	Comment	Design Code Update Response
RE Cycle Parking and Car Parking 2 & 3	"RE 2 and RE 3 2 set out requirements for cycle and car parking that are different to those in the Local Plan. For the avoidance of confusion and to ensure that sufficient spaces are provided it is suggested that the text for each is revised."	Disagree - There are no requirements for cycle parking within the Local Plan so we would want to keep this in to enable a culture of cycling
RE4 Green Spaces	Revised to reflect the important role of front gardens.	Disagree - as current wording allows for deep front gardens and has been updated with coding to provide deep back gardens (15m deep).
RE8 Building Line	Re-worded to allow for some streets with homes closer to the pavement on one side only.	Disagree - there is flexibility in the coding already for variation from the building line.
RE9 Height	Make all buildings two storey	Disagree - this is already part of the coding.
RE11 Identity	It is not considered appropriate to prescribe that the architectural design approach should be 'a contemporary interpretation of local vernacular' especially in the absence of any evidence for why this is the best design approach and/or any evidence that this is the preference of the wider community. Slate and clay tiles are very expensive and a high quality architectural treatment can be achieved without using natural materials for roof coverings. There is no evidence for why 'vertical windows' are appropriate here as this is not a characteristic of High Halstow or other local villages. Homes of an Arts and Crafts inspired design are characteristic on the rural edges of High Halstow and other villages such as Cliffe.	The wording has been updated for more flexibility on new development to respect local vernacular.
Additional Area Type	Provide an additional area type.	Three character areas have been agreed with the Parish Council.

